

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:	State Road (SR) 26 over Salamonie River, Jay County
Designation Number(s):	1600828
Project Description/Termini:	SR 26 over Salamonie River crossing improvement, approx. 0.78 mile east of United States (US) 27

	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
X	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

_____	_____
INDOT DE Signature and Date	INDOT ESD Signature and Date

FHWA Signature and Date	

Release for Public Involvement

<u>TD</u>	<u>7/14/2021</u>	<u>REB</u>	<u>7-20-2021</u>
INDOT DE Initials and Date		INDOT ESD Initials and Date	

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Erin Mulryan, SJCA Inc.

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Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input checked="checked" type="checkbox"/>	<input type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on December 4, 2017 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G-1.

This project involves Bridge No. 026-38-03430A [National Bridge Inventory (NBI) No. 007040], which carries SR 26 over the Salamonie River in the City of Portland, Wayne Township, Jay County, Indiana. The bridge is listed in the Indiana Department of Transportation (INDOT) Historic Bridge Inventory Collection as eligible for the National Register of Historic Places (NRHP) under Criterion C because it “represents an early or distinctive phase in bridge construction, design or engineering and it retains the historic integrity necessary to convey its engineering significance”. Furthermore, it is a good example of an Indiana State Highway Commission (ISHC) designed Parker through truss. It is one of six (6) or fewer examples within a district of INDOT. In addition, INDOT Cultural Resources Office (CRO) staff conducted an aboveground resources investigation for the SR 26 pavement replacement project under Des. No. 0100715 in 2008, which recommended one property, Bridge No. 026-38-03430A (NBI No. 007040), the bridge in this project, as eligible for the NRHP under Criterion C (Appendix D-14). The bridge is classified as a “Non-Select” bridge by the INDOT Historic Bridge Inventory Collection. Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA, or HBPA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its responsibilities involving “Select” and “Non-Select” bridges through the Project Development Process (PDP) of the HBPA (Stipulation III) to comply with Section 106 of the National Historic Preservation Act (NHPA) of 1966.

To meet the public involvement requirements of Section 106, a legal notice of the FHWA finding of “No Historic Properties Affected” was published in the *Commercial Review* on March 6, 2021 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The published public comment period end date was April 7, 2021. The text of the public notice and the affidavit of publication appear in Appendix D-1 to D-2. No comments from the public were received during the comment period ending on April 7, 2021.

Per the marketing requirements of the HBPA, on March 9, 2020, two public notices were published in the *Indianapolis Star* and *The Commercial Review*, the daily newspaper of Jay County, Indiana, offering the bridge to interested responsible parties for the rehabilitation and reuse, the storage and future reuse, or salvage elements of the bridge. The bridge was placed on the INDOT Historic Bridges Marketing Program website (<https://www.in.gov/indot/2532.htm>) on February 12, 2020, and marketing signs were posted on the west and east sides of the bridge. The bridge is currently listed as “pending” on the INDOT Historic Bridge Marketing website. No sponsors to take responsibility of the relocation and preservation of the bridge have been identified as of the date of this Categorical Exclusion (CE) document, and the bridge is still currently listed on the INDOT bridge marketing website as “pending.” Refer to Appendix D-71 to D-76 for all marketing documents.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Pursuant to the HBPA, a public hearing is required. A legal notice will appear in a local publication contingent upon the release of this CE document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

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Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Greenfield

Local Name of the Facility: SR 26 over Salamonie River

Funding Source (mark all that apply): Federal ☒ State ☒ Local ☐ Other* ☐

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need:

The need for the project is that the existing bridge does not meet current INDOT design criteria for capacity or shoulder width. Based on the August 30, 2017, Bridge Inspection Report by USI Consultants, Inc., the deck, wearing surface, superstructure, substructure, and channel/channel protection are rated 5 out of 9 (fair condition). This rating scale provides a numerical value to the conditions of various components of bridge and structures such as wearing surfaces, superstructures, and channel conditions, with 0 out of 9 being the worst scenario (failed conditions) and 9 out of 9 being the best scenario (excellent conditions). For more information, refer to the FHWA's *Recording and Coding Guide for the Structure Inventory and Appraisal of the Nation's Bridges* at <https://www.fhwa.dot.gov/bridge/mtguide.pdf>. Refer to Appendix J-61 to J-63 for pages of the August 30, 2017, Bridge Inspection Report.

Deficiencies of the structure include:

- Capacity: The bridge, built in 1941, was originally designed to carry vehicles up to 20 tons (H-20 inventory rating) but due to the structure's deterioration, current loads are limited to 16 tons (H-20 inventory rating)/28 tons (HS-20 inventory rating). Refer to <https://www.fhwa.dot.gov/reports/tswstudy/Vol3-Chapter6.pdf> for more information about bridge load ratings. Bridges typically have multiple load ratings based on its capacity to carry different types of large vehicles such as fire trucks and school busses, which have different axle loads, axle spacings, and other size dimensions and weight distributions. The deterioration of the structure has led to a reduction in the bridge load limit; currently semi-tractor trailers, grain haulers, large farm equipment, and large emergency vehicles such as fire trucks are prohibited from using the structure. Deficiencies of the structure include (Appendix J-4 to J-7):
 - Cracking of the bridge deck and wearing surface.
 - Non-standard bridge railing with corrosion at the connections and section loss holes at the southeast and northwest corners.
 - Spalls and exposed reinforcement in the curbs and sidewalks.
 - Minor to moderate section loss to flanges and webs of the fascia stringers in the end panels, primarily at the stringer connections to floor beams; defects are primarily on the exterior face of the fascia beams.
 - Pitting, rust, and/or deteriorations at the ends of the lower lateral bracing gusset plate connections of the floor beams.
 - Minor corrosion, pitting, and section loss of several diagonal members.
 - Pitting, corrosion, and minor to moderate section loss of the lower chords.
 - Corrosion and major section loss of upper chords and end posts.
 - Pitting, corrosion, and section loss of the vertical gusset plates; some of the gusset plates are deformed due to pack rust.
 - Moderate corrosion and section lost of horizontal connection plates, especially at the southeast end post, with pack rust causing some distortion at most locations; all lower lateral bracing gusset plates have pack rust and deformation at connections.

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- Almost all diagonals, verticals, and lower chord members are fracture critical; members are either tension or subjected to stress reversal.
- Minor impact damage and scrapes along the bridge rail.
- The concrete support block for the east end floor beam has spalled in the support area; steel bearings are rusted but functional.
- The abutments are in fair condition with horizontal and vertical cracks, delamination, and spalls along the joint between the original concrete and the repairs made in 1979.
- Minor vertical cracks in the concrete bridge seats and mudwalls.
- Erosion and undermining at the corners of the abutments.
- Random cracks and minor rutting of approaches
- Wide longitudinal cracks along the center construction joint.
- Substandard and leaning approach guardrails.

The nature and volume of existing and proposed traffic on SR 26 necessitates that the bridge be capable of safely carrying modern highway loadings (36-ton vehicles) including commercial vehicles, grain haulers, school buses, and emergency vehicles.

- Roadway width: The bridge roadway carries two 11-foot lanes with two (2) foot wide shoulders on each side of the roadway. Current INDOT design criteria require a minimum lane width of 11 feet with a desired width of 12 feet, and minimum shoulder width of three (3) feet with a desired width of eight (8) feet. Although the driving lane width meets minimum width criteria, the shoulders do not.

Purpose:

The purpose of the project is to improve the rating of the deck, wearing surface, superstructure, substructure, and channel/channel protection to at least a 7 (satisfactory) out of 9 or better and to improve the carrying capacity of the bridge from the current 16 tons (H-20 inventory rating)/28 tons (HS-20 inventory rating) to 20 tons (H-20 inventory rating)/36 tons (HS-20 inventory rating) so the structure can accommodate agricultural and emergency equipment currently prohibited from using the existing structure.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Jay Municipality: City of Portland

Limits of Proposed Work: From a point approximately 0.68 mile east of US 27 to approximately 0.87 mile east of US 27

Total Work Length: 0.196 Mile(s) Total Work Area: 1.5 Approx. Acre(s)

Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Yes¹

No

☐

☒

Date:

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

Location

This project is located on the east side of the City of Portland on SR 26 in Wayne Township, Jay County, approximately 0.78 mile east of US 27 in Section 21, Township 23 North, Range 14 East as shown on the Portland, Indiana 1:24,000 quadrangle map. Project location maps can be found in Appendix B-1 to B-3.

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Existing Conditions

The existing roadway of SR 26 within the project area is classified as a Rural Major Collector and is a two-lane roadway constructed of asphalt. The lanes are 11 feet in width with two (2) foot wide paved shoulders. Approximately 370 feet east of the bridge, the paved shoulder widens on the north side of SR 26 to 11 feet to allow for traffic passage around farm equipment turning left into the existing driveway on the south side of the roadway (refer to the aerial map in Appendix B-3 and site photo in Appendix D-20). Asphalt chip and seal approaches are on the east and west sides of the existing bridge structure. Aluminum guardrails exist on both sides of the bridge along the approaches. The posted speed limit is 40 miles per hour (MPH). There are no pedestrian facilities along SR 26 within the project area. There are four (4) existing drainage structures in the project area that convey stormwater from the roadside ditches under roadways and agricultural field access drives (see site photos in Appendix F-30 to F-36). One (1) gravel residential driveway is located approximately 175 feet west of the western bridge approach on the north side of SR 26. One (1) concrete entrance to the Jay-Randolph Developmental Services and recycling facility is located approximately 240 feet west of the western bridge approach on the south side of SR 26. One (1) asphalt agricultural access drive is located approximately 415 feet east of the eastern bridge approach on the south side of SR 26.

INDOT Bridge No. 026-38-03430 A (NBI No. 007040) is a single span, metal Parker through truss structure built in 1941 by the Yost Brothers of Decatur, Indiana. The bridge carries SR 26 over the Salamonie River at a zero-degree skew. The bridge has a span of 150 feet, clear roadway width of 28 feet (two 11-foot-wide lanes with two-foot-wide shoulders), out-to-out coping width of 29 feet, and an out-to-out bridge floor of 154 feet, 8.5 inches in length. The existing bridge has a concrete deck, non-standard steel bridge railings, and a vertical clearance of 14.64 feet. The bridge was included in the Indiana Historic Bridge Inventory (HBI), conducted by Mead & Hunt on INDOT's behalf, as a Non-Select bridge. The bridge is eligible for the NRHP under Criterion C as one of six (6) or fewer examples of this bridge type within the INDOT Greenfield District. The bridge was painted in 2000, repaired in 1979, and the deck was replaced in 1975.

The project bridge crosses the Salamonie River. The surrounding terrain is generally flat. The existing conditions surrounding the side slopes and underside of the bridge structure consist of riparian vegetation and trees. Maintained roadside grasses exist adjacent to the SR 26 roadway east and west of the bridge. East of the bridge, agricultural fields exist in the project vicinity beyond the riparian corridor of the Salamonie River and the SR 26 roadway. Residences, an apartment complex, East Elementary School, and Jay-Randolph Developmental Services and recycling facility are located west of the bridge. There is a system of trails with exercise stations southwest of the bridge (see plan sheet in Appendix B-15 and site photo in Appendix B-9). There are several utilities in the project area including overhead power, communication, and telephone lines and buried fiber optic lines.

Preferred Alternative

The Historic Bridge Alternatives Analysis (HBAA), completed in February 2020, evaluated six (6) alternatives to determine what option was feasible and prudent, and met the purpose and need of the project, which are discussed in more detail below in the Other Alternatives section:

- Alternative A: Do Nothing
- Alternative B: Rehabilitation of Existing Structure for Continued Vehicular Use (two-lane option) Meeting Secretary of Interior's Standards for Rehabilitation
- Alternative C: Rehabilitation of Existing Structure for Continued Vehicular Use (one-way pair option) Meeting Secretary of Interior's Standards for Rehabilitation with Construction of New One-Way Structure
- Alternative D: Bypass (non-vehicular use) / Build New Structure without Affecting the Historic Integrity
- Alternative E: Relocation of Historic Bridge and New Bridge Construction
- Alternative F: Replacement – Demolition of Historic Bridge and New Bridge Construction

The full HBAA can be found in Appendix J-2 to J-142. The Indiana State Historic Preservation Officer (SHPO) responded on March 30, 2020 after their review of the HBAA, stating that Alternatives A, B, C, and D are feasible but not prudent based on need and costs, and agreed that Alternative E is prudent only if a responsible party steps forward to fund the relocation of the bridge, rehabilitation, and maintenance of the bridge. The SHPO went on to state that "should a responsible party not step forward during the bridge marketing period, we are satisfied that Alternative F: Replacement-Demolition of Historic Bridge and New Bridge Construction is the only remaining alternative that is both feasible and prudent" (Appendix D-48 to D-50).

No sponsors to take responsibility of the relocation and preservation of the bridge have been identified as of the date of this CE document. Therefore, Alternative F, the preferred alternative of this project, will involve the demolition of the existing bridge and the construction of a replacement bridge meeting all current INDOT design criteria along the existing alignment. The estimated construction cost of the replacement structure included in the HBAA is approximately \$1,158,300; a breakdown of this cost is included in Appendix J-43. The estimated cost of construction in the Indiana State Transportation Improvement Program (STIP) is \$1,923,370. The cost differences between the HBAA and STIP are due to modifications in design after the HBAA was compiled. The

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design for the bridge was widened to include sidewalks on both sides of the bridge; the design and related costs in the HBAA did not include sidewalks. The length of the new bridge was extended from 200 linear feet to 210 linear feet. In addition, construction costs fluctuate with the costs of materials and labor.

The replacement structure will be a continuous composite prestressed concrete bulb tee beam structure and will consist of three spans at 70 feet for a total of 210 linear feet in length to provide adequate hydraulic capacity for the crossing. The replacement structure will have a 15-degree skew. The typical section will consist of two 11-foot-wide travel lanes with a 4-foot, 4-inch shoulder on the north side of the structure and a 3-foot, 8-inch shoulder on the south side (on the bridge only) for a clear roadway of 30 feet. The out-to-out coping width of the new structure will be 38 feet, 10 inches. Refer to Appendix B-20 for the proposed typical bridge section. The new structure will have a load rating of 20 tons (H-20 inventory rating)/36 tons (HS-20 inventory rating). The existing asphalt roadway within the project limits will be milled and resurfaced and will maintain the existing 11-foot-wide lane widths. New roadway shoulders will vary from one (1) foot to four (4) foot; refer to Appendix B-12 for roadway typical sections. A 6-foot, 10-inch concrete sidewalk is proposed on the south side of the replacement structure, and a 6-foot, 3-inch sidewalk is proposed from the west end of the new structure on the south side of SR 26. Two (2) new pipes to convey stormwater and two (2) new end bent pipes will also be installed (refer to the Bridges and Small Structures section below for more details). Riprap will be installed along the banks of the Salamonie River under the new structure and around the new piers. The approximate project length for this alternative is 1,000 feet along SR 26. Above and below ground utilities in the project area will likely require relocation. Preliminary plans can be found in Appendix B-10 to B-22.

Based on current estimates, the project will require approximately 0.731 acre of permanent right of way (ROW); no temporary ROW is proposed. All ROW acquisition is planned for the south side of SR 26 and the project bridge to avoid impacts to the unnamed cemetery in the northwest quadrant.

The maintenance of traffic (MOT) will involve closure of SR 26 and a detour utilizing SR 49, SR 67, and US 27/SR 67. Refer to the MOT section below. The project is scheduled for letting in April 2022.

The preferred alternative meets the purpose and need of the project by providing a crossing carrying SR 26 over Salamonie River that can meet the current INDOT design standards for vehicular traffic of HS-20 (36 tons) or better to accommodate commercial truck and agricultural equipment traffic. The new structure will have sufficient width and load rating to accommodate such equipment without restrictions. The new bridge will have a deck, wearing surface, superstructure, substructure, and channel/channel protection rating of at least a 7 out of 9 (satisfactory) or better.

Logical Termini/Independent Utility:

The limits north and south of the project bridge are such that access for removal of the existing structure and construction of the new bridge and installation of scour protection will be provided. The roadway limits to the west of the bridge are such that the existing pavement removed for construction will tie into the existing pavement at the east side of the entrance to Jay-Randolph Developmental Services, thus avoiding impacts to the entrance. The roadway limits to the east of the bridge are such that the passing shoulder on the north side of SR 26 will be repaved to match the new pavement removed during construction. The new sidewalk on the north side of the bridge will provide for future connections to pedestrian facilities; by constructing this sidewalk with this project, future impacts to the bridge when construction pedestrian facilities will be minimized. The new sidewalk on the west side of the bridge will terminate at the driveway to Jay-Randolph Developmental Services and recycling facility. The new sidewalk on the east side of the bridge will terminate with an Americans with Disabilities Act (ADA) compliant transition at the end of the new guardrail, providing enough distance from the new bridge to allow for the construction of connections to future pedestrian facilities without impacts to the bridge. The new sidewalk along the south side of the SR 26 roadway and new bridge will also provide pedestrians an ADA compliant walking route to view wildlife and the Salamonie River from the bridge. The project has independent utility because it doesn't rely on another project to meet its purpose and need. Therefore, the project has logical termini and independent utility.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

The HBAA investigated five (5) alternatives besides the preferred alternative. Refer to Appendix J for the HBAA.

Alternative A: Do Nothing

Alternative A is an avoidance alternative that would allow the existing structure to remain in place with no improvements. INDOT

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would continue its current inspection program to identify structural deficiencies and would address issues as required. This alternative would not use federal funds and no action would occur. The structure would continue to deteriorate. Without repairs to the deteriorating lower chord members and gusset plates and a new paint system to seal and slow corrosion, the bridge would probably require posting for load within the next three (3) to five (5) years. Should this structure become un-useable, a three (3) mile detour consisting of moderate volume roads is available.

With the bridge in its current condition, this alternative fails to meet the stated purpose and need for a structurally safe and sufficient bridge.

Alternative B: Rehabilitation of Existing Structure for Continued Vehicular Use (two-lane option) Meeting Secretary of Interior's Standards for Rehabilitation:

This alternative would consist of rehabilitating the existing structure in accordance with the Secretary of Interior's Standards for Rehabilitation or as close to the Secretary's Standards as is practicable. Refer to the display of Alternative B in Appendix J-25. The structure would continue to accommodate two-way traffic. The existing bridge would be repaired as necessary. Approach guardrail would be replaced with railing meeting current design standards. Refer to Figure B in Appendix J-9 for a summary of the bridge's existing design elements and applicable design criteria.

The bridge railing does not meet FHWA or INDOT current design criteria, is not crash tested and would require a design exception to be left in place. Per the Indiana Design Manual (IDM), article IDM 55-6.02 railing may be left in place only if the following conditions are met:

- a. The project is a rehabilitation project on a non-NHS route
- b. The existing bridge railing and approach guardrail are considered to be satisfactory
- c. The accident history does not indicate that there may be a problem
- d. The design year annual average daily traffic (AADT) is less than 400; and
- e. The design speed is 30 miles per hour (mph) or lower.

Since conditions b (rail is in fair condition), d (AADT is 4010 vehicles per day), and e (design speed is 40 mph) are not met, a design exception would not be granted. The existing bridge rail would be removed and replaced with an FC type barrier to meet current safety requirements.

Level 1 design exceptions would be required for inadequate lane width and inadequate width of shoulder. Since the bridge clear roadway and the approach roadway are both 28 feet, a design exception to leave the current travel lane and shoulder width would likely be granted.

No additional ROW would be required for this alternative. Since the work would be performed over a waterway, various permits would be required. With a drainage area of approximately 46 square miles, this project would require an Indiana Department of Natural Resources (IDNR) Construction in a Floodway (CIF) permit. An Indiana Department of Environmental Management (IDEM) Section 401 Water Quality Certification (WQC) and a U.S. Army Corps of Engineers (USACE) Section 404 permit would be required if any work is to be performed below the Ordinary High Water Mark (OHWM). An IDEM Rule 5 permit is not anticipated since the disturbed area would likely be less than one acre for the rehabilitation project.

A review of the fracture critical inspection and the current load rating analysis shows that the following members contribute to the insufficient load capacity:

- South Truss - Lower Chord member L0L1 – Heavy corrosion and pitting of the member within the end 1'-0" of the beam.
- South Truss – Lower Chord member L6L7 – Heavy corrosion and moderate section loss of the end of the beam below the southeast end post
- North Truss - Deteriorated gusset plate at Panel Point L3.
- Rivets in the gusset plates have lower capacity than the truss members they connect:
 - U1 and U6 (vertical members U1L1 and U6L6) in both trusses.
 - U1 and U6 (diagonals U1L2 and L5U6)

Refer to Appendix J-10 for a table of the load rating results under damaged conditions.

Repair or replacement of the deteriorated truss members with similar strength steel of the same size and replacing existing rivets with high strength bolts in key locations would bring the bridge to compliance with the structural capacity criteria and would meet the Secretary of Interior's Standards for Rehabilitation. Refer to Appendix J-11 for a table of the load rating results under repaired conditions.

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Substructure repairs for this alternative would include repairs to the abutments including removing loose concrete, cleaning exposed reinforcement and patching the concrete.

Additional repairs to the superstructure include a full deck replacement (existing deck is 40 years old), replacing missing lacing bars at the endposts, replacing approximately ten percent (10%) of the stringers due to deterioration; replacing the existing bridge rail with FC rail, and cleaning and painting the entire structure. The current paint system is approximately 20 years old. Since the most recent painting was in 2000, the paint in place is probably not lead based paint.

SR 26 over the Salamonie River, built in 1941 by the Yost Brothers of Decatur, Indiana is an example of an ISHC standard plan for a moderately-long span bridge. This version of the standard plans relied heavily on rolled I-beams in the webbing and lower chord members. Replacement or repair of damaged members would have minimal impact on the overall appearance of the structure. Only two lower chord members are proposed for replacement. Stringers are not considered "character defining" members. No significant changes to the historic character defining members of the bridge are proposed.

The most significant component of rehabilitating the existing bridge is the cost of cleaning and painting. Cleaning the bridge, including collection and disposal of the removed paint, protection of the Salamonie River, and painting the bridge, are anticipated to cost between \$350,000 and \$400,000.

The estimated cost to rehabilitate the existing bridge is \$925,300.00 (Note: the cost estimate in Appendix D of the HBAA is \$962,286.62; see Appendix J-31 of this CE document). Preliminary costs for a replacement bridge along the existing alignment are \$1,158,300.00 (Appendix J-43), making rehabilitation costs approximately 80 percent of replacement costs. In addition, the steel through truss requires special inspection procedures and equipment for fracture critical members and fatigue sensitive details.

Although most minimum design standards can be met and design exceptions for insufficient travel lane and shoulder width would likely be granted, this alternative is not prudent for a Non-Select structure since initial rehabilitation costs are 80 percent of the initial replacement costs.

Since the repairs described in Alternative B, with design exceptions, meet the Secretary of Interior's Standards, Alternative B2 (not meeting the Secretary of Interior's Standards) were not investigated.

Alternative C: Rehabilitation of Existing Structure for Continued Vehicular Use (one-way pair option) Meeting Secretary of Interior's Standards for Rehabilitation with Construction of New One-Way Structure with Construction of New One-Way Structure:

This alternative would consist of rehabilitating the existing structure in its current configuration, accommodating one-way traffic and constructing a new one-way structure. This alternative would rehabilitate the existing truss structure for continued vehicular use with one lane of traffic and would require the same repairs to the existing structure as noted in Alternative B. Since the repairs described in Alternative B meet the Secretary of Interior's Standards, Alternative C2 (not meeting the Secretary of Interior's Standards) will not be investigated.

In addition to rehabilitating the existing structure, a new three-span, one-way structure would be constructed to the north of the existing structure on a parallel alignment (refer to the display in Appendix J-27). The new bridge would be designed for future two-way use and would meet all current INDOT design criteria. The new bridge is assumed to consist of three spans at 50 feet, 100 feet, and 50 feet to provide adequate hydraulic capacity for the crossing.

Since the work would be performed over a waterway, various permits would be required. With a drainage area of approximately 46 square miles, this project would require an IDNR CIF permit. An IDEM Section 401 WQC permit, a USACE Section 404 permit would be required if any work would be performed below the OHWM, and an IDEM Rule 5 permit would also likely be required.

The new one-way bridge would require approximately 0.636 acre of additional ROW. The ROW required is currently occupied by farm fields, forested areas and residential properties. The estimated cost of purchasing additional ROW is approximately \$15,000 based on property value only.

The approximate project length for this alternative is 1,200 feet long. The new bridge was assumed to be a three-span concrete structure with prestressed bulb tee beams for this analysis. The estimated construction cost of a new one-way parallel structure is approximately \$1,343,000. The total estimated cost, including ROW, for Alternative C is \$1,358,000. Refer to Appendix J-37 for a cost summary of Alternative C, excluding ROW costs.

This alternative would include the cost of rehabilitating the existing truss in addition to the cost of a new bridge (Alternative F) on a

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new roadway alignment and ROW acquisition. Although this alternative is feasible it is not prudent.

Alternative D: Bypass (non-vehicular use) / Build New Structure without Affecting the Historic Integrity:

This alternative would consist of rehabilitating the structure for pedestrian use in accordance with the Secretary of the Interior's Standards for Rehabilitation or as close to the Secretary's Standards as practicable and per the HBPA Section 4(f) evaluation.

The existing bridge would be repaired as described in Alternative B. In addition to rehabilitating the existing structure, a new three-span, two-way bypass structure would be constructed to the north of the existing structure on a parallel alignment (refer to the display in Appendix J-27). The new bridge would be designed to meet all current INDOT design criteria. The new bridge is assumed to consist of three spans at 50 feet, 100 feet, and 50 feet to provide adequate hydraulic capacity for the crossing. The typical bridge cross section would consist of two 11-foot travel lanes adjacent to 4-foot-wide shoulders for a clear roadway width of 30 feet. Bridge railing would be type FC bridge railing. The out-to-out width at the bridge coping would be 33 feet.

Since the work would be performed over a waterway, various permits would be required. With a drainage area of approximately 46 square miles, this project would require an IDNR CIF permit. An IDEM Section 401 WQC permit, a USACE Section 404 permit if any work is to be performed below the OHWM, and an IDEM Rule 5 permit would also be required for this project.

The new bypass bridge structure would require approximately 0.636 acre of additional ROW. The ROW required is currently occupied by farm fields, forested areas and residential properties. The estimated cost of purchasing additional ROW is approximately \$15,000 based on property value only.

The approximate project length for this alternative is 1,200 feet long. The new bridge was assumed to be a three-span concrete structure with prestressed bulb tee beams for this analysis. The estimated construction cost of a new two-way bypass structure is approximately \$1,343,000. The total estimated cost, including ROW, for Alternative D is \$1,358,000. Note, the cost of rehabilitation of the existing bridge is not included in this alternative since the HBPA states that a responsible party other than the owner must come forward before the end of the public hearing comment period to assume liability and fund preservation and maintenance of the bridge for this alternative to be feasible.

The new construction cost is 117% of the cost for replacement (Alternative F). For a Non-Select bridge, this alternative is prudent only if a responsible party other than the owner comes forward to fund the rehabilitation and maintenance of the bridge.

Alternative E: Relocation of Historic Bridge and New Bridge Construction:

Alternative E would consist of relocating and rehabilitating the structure for pedestrian use in accordance with the Secretary of the Interior's Standards for Rehabilitation or as close to the Secretary's Standards as practicable and per the HBPA Section 4(f).

In addition to relocating and rehabilitating the existing structure, a new three span, two-way structure would be constructed on the existing alignment. The new structure would be a two-lane structure consisting of three spans at 50 feet, 100 feet, and 50 feet to provide adequate hydraulic capacity for the crossing. The typical bridge cross section would consist of two 11-foot travel lanes adjacent to 4-foot-wide shoulders for a clear roadway width of 30 feet. Bridge railing would be type FC bridge railing. The out-to-out width at the bridge coping would be 33 feet. With FC railing, the out to out at the coping of bridge would be 33 feet. The approximate project length for this alternative is 1,000 feet along SR 26.

Since there would be performed over a waterway, various permits would be required for the project. These include a Certificate of Approval for Construction in a Floodway (drainage area of 46 square miles), a Section 401 IDEM permit and a Section 404 USACE permit. An IDEM Rule 5 permit is not anticipated since the disturbed area would likely be less than one acre for the replacement project.

The estimated construction cost of the replacement structure is approximately \$1,158,300. No additional ROW would be required for this alternative. The existing structure, in accordance with INDOT's Cultural Resource Manual, Chapter 2-1.0, would be advertised for a minimum period of six months to allow any interested individual(s) or group(s) the opportunity to assume responsibility for the bridge and fund the relocation, rehabilitation and maintenance of bridge.

Alternative E is feasible, meeting all current INDOT design standards. For a Non-Select bridge, this alternative is prudent only if a responsibility party other than the owner comes forward to fund the relocation, rehabilitation and maintenance of bridge. Refer to Appendix J-28 for a display of Alternatives E and F.

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The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway SR 26
 Functional Classification: Rural Major Collector
 Current ADT: 2902 VPD (2022) Design Year ADT: 3984 VPD (2042)
 Design Hour Volume (DHV): 438 Truck Percentage (%) 16
 Designed Speed (mph): 40 Legal Speed (mph): 40

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Asphalt through lanes		Asphalt through lanes	
Pavement Width:	11	ft.	11	ft.
Shoulder Width:	2	ft.	1-4 (roadway)	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A (within project area)	ft.	6 ft, 3 in. (6 ft, 10 in. on bridge)	ft.

Setting: ☒ Urban ☐ Suburban ☐ Rural
 Topography: ☒ Level ☐ Rolling ☐ Hilly

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BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): INDOT Bridge No. 026-38-03430A (NBI No. 007040) Sufficiency Rating: 63.6; 8/30/2017 Bridge Inspection Report; Appendix J-61 to J-63) (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:	Parker metal through truss	Continuous composite prestressed concrete bulb tee beam
Number of Spans:	1	3
Weight Restrictions:	28 (HS Inventory) 16 (H Inventory)	36 (HS Inventory) 20 (H Inventory)
Height Restrictions:	14.64	N/A
Curb to Curb Width:	28	30
Outside to Outside Width:	31.54	38.83
Shoulder Width:	2	4 ft, 4 in. (north side, bridge only); 3 ft, 8 in. (south side, bridge only)

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

INDOT Bridge No. 026-38-03430A (NBI No. 007040) is a Non-Select, single span, metal Parker through truss structure built in 1941 by the Yost Brothers of Decatur, Indiana. The existing bridge is eligible for the NRHP under Criterion C as an example of an ISHC standard plan for a moderately-long span bridge. This version of the standard plans for construction relied heavily on rolled I-beams in the webbing and lower chord members. Refer to Appendix J-20 to J-23; J-70 to J-71; and J-87 to J-107 for photos of the bridge.

Based on the results of the HBAA, Alternative E would consist of relocating and rehabilitating the structure for pedestrian use in accordance with the Secretary of the Interior's Standards for Rehabilitation or as close to the Secretary's Standards as practicable and per the HBPA Section 4(f). If disassembly and storage of the historic bridge is not pursued, then Alternative F, which involves the demolition of the historic bridge and new bridge construction, will also meet the purpose and need of the project and will be the preferred alternative. The new bridge construction involves a 210-foot-long, three span prestressed concrete bulb tee beam structure with spans of 70 feet each, and the bridge will have 15 degrees of skew. The typical section will consist of two 11-foot-wide travel lanes with a 4-foot, 4-inch shoulder on the north side of the structure and a 3-foot, 8-inch shoulder on the south side for a clear roadway of 30 feet. A 6-foot, 10-inch concrete sidewalk is proposed on the south side of the replacement structure. The out-to-out coping width will be 38 feet, 10 inches. Two (2) 6-inch end bent drain pipes will be installed on the new structure as shown in the plan sheet in Appendix B-19. "H" and "HS" Inventory refer to the design load of the bridge.

One (1) new 12-inch pipe approximately 25 feet in length will be installed at the western terminus of the project under the SR 26 roadway. It will connect to a new 12-inch pipe approximately 162 feet in length along the south side of SR 26 to the southwest quadrant of the bridge. The purpose of these pipes is to convey stormwater under and along the south side of the roadway.

There are four (4) existing drainage structures in the project area that convey stormwater (see site photos in Appendix F-30 to F-36; these structures are identified as "culverts" in the maps in Appendix F-17 and F-18):

- One (1) 12-inch reinforced concrete pipe (RCP) located on the north side of SR 26, west of the bridge, that conveys stormwater under a gravel drive and outlets near Salamonie River
- One (1) 24-inch corrugated metal pipe (CMP) under the access drive to the agricultural field in the northeast quadrant of the project bridge and conveys stormwater under the drive
- One (1) 15-inch CMP under the access drive to the agricultural field in the southeast quadrant of the project bridge and

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- conveys stormwater under the drive
- One (1) 15-inch CMP partially buried, with an outlet near the eastern bank of the Salamonie River on the south side of the project bridge (not shown in the preliminary plans in Appendix B-15; refer to aerial map in Appendix F-17 and site photo in Appendix F-36)

Refer to Appendix B-15 to B-16 for these existing structure locations. No work is proposed to these structures; they will remain in place. No other bridges or small structures are located within the project area.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?		X
Will the project involve the use of a detour or require a ramp closure? (describe below)	X	
Provisions will be made for access by local traffic and so posted.	X	
Provisions will be made for through-traffic dependent businesses.	X	
Provisions will be made to accommodate any local special events or festivals.	X	
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X

Discuss closures and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require a closure and a detour utilizing SR 49, SR 67, and US 27/SR 67. The closure is expected to last five (5) months. The approximate length of the detour is 37 miles, and the additional travel time is approximately 60 minutes. Access to all properties will be maintained throughout construction.

The Tri-State Gas Engine and Tractor Show Association was contacted on January 22, 2021 regarding impacts the proposed detour and closure may have to events the organization holds annually at the Jay County Fairgrounds. The organization responded on February 22, 2021 stating that their show is in August and swap meet is in May, therefore they did not have any concerns since construction is proposed from October 2022 to April 2023 (Appendix C-33).

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion. Delays would occur during construction but will cease with project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 250,000 (2018) Right-of-Way: \$ 50,000 (2021) Construction: \$ 1,923,370 (2022)

Anticipated Start Date of Construction: October 2022

Note: an update to the STIP will be required to reflect current costs. Costs vary between the HBAA and the STIP due to modifications in design after the HBAA and fluctuation in costs of materials and labor.

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0	0
Commercial (Jay-Randolph Developmental Services and recycling facility)	0.06	0
Agricultural	0.001	0
Forest (riparian corridor)	0.56	0
Wetlands	0.11	0
Other:	0	0
Other:	0	0
TOTAL	0.731	0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

This project will require approximately 0.731 acre of permanent ROW, all from the south side of the SR 26 roadway and project bridge. No temporary ROW is proposed. Approximately 0.06 acre will be required from the Jay-Randolph Developmental Services and recycling facility in the southwest quadrant of the bridge. Approximately 0.56 acre will be acquired from the forested riparian corridor of the Salamonie River along the south side of the bridge. Approximately 0.11 acre of Wetland A, which is located along the south side of the SR 26 roadway east of the bridge, will be required (refer to Appendix F-17 for the location of Wetland A). Approximately 0.001 acre of agricultural land will be acquired east of Wetland A and along the south side of SR 26, near the eastern project terminus. All ROW acquisition is planned for the south side of SR 26 and the project bridge to avoid impacts to the unnamed cemetery in the northwest quadrant. Refer to Appendix B-15 for the location of the cemetery and existing and proposed ROW acquisition.

The typical existing ROW width varies throughout the project area between 11 feet to 60 feet from the centerline of the roadway on the north side and 11 feet to 40 feet from the centerline of the roadway on the south side. Maximum ROW width on the south side of the roadway will vary from approximately 40 feet to 55 feet from the centerline of the roadway. ROW widths from the centerline will not change on the north side of the roadway.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on August 12, 2020, August 25, 2020, January 11, 2021, and January 22, 2021. Refer to Appendix C-1 to C-5.

Agency	Date Sent	Date Response Received	Appendix Page
FHWA	August 25, 2020	No response received	N/A
IGS	Automated letter	Generated on January 10, 2021	C-18 to C-20
Indiana Dept. of Environmental Management (IDEM) Groundwater Section	August 12, 2020	August 25, 2020	C-6
Indiana Department of Natural Resources (IDNR) Division of Fish & Wildlife	August 25, 2020	September 24, 2020	C-15 to C-17
IDEM On-line Submission	Automated letter	Generated on January 11, 2021	C-21 to C-27
INDOT Manager, Public Hearings	August 25, 2020	No response received	N/A
US Dept. of Housing & Urban Development (HUD)	August 25, 2020	No response received	N/A
National Park Service (NPS)	August 25, 2020	No response received	N/A
INDOT Greenfield District	August 25, 2020	August 27 & September 1, 2020	C-9
USFWS	January 11, 2021	January 11 & 25, 2021	C-28 to C-29
Natural Resource Conservation Service (NRCS)	August 25, 2020; April 5, 2021	April 9, 2021	C-13, C-14
INDOT Aviation	August 25, 2020; May 17, 2021	August 26, 2020; May 18, 2021	C-7; C-34
US Army Corp of Engineers (USACE)	August 25, 2020	No response received	N/A
US Coast Guard (USCG)	August 25, 2020	September 3, 2020	C-12
City of Portland Mayor	August 25, 2020	No response received	N/A
Jay County Surveyor	August 25, 2020	No response received	N/A
Jay County Commissioner	August 25, 2020	No response received	N/A
Jay County Schools- Transportation	August 25, 2020	August 26, 2020	C-8
East Elementary School	August 25, 2020	No response received	N/A
Jay Co. Floodplain Administrator	August 25, 2020	No response received	N/A
MS4 Coordinator, City of Portland	August 25, 2020	No response received	N/A
Portland Municipal Water Plant	August 25, 2020	No response received	N/A
City of Portland Parks (Superintendent of Streets and Parks)	August 25, 2020	No response received	N/A
Jay Co. Visitors & Tourism Bureau	January 22, 2021	No response received	N/A
Tri-State Gas Engine & Tractor Show Association	January 22, 2021	February 22, 2021	C-32
Jay Co. Fairgrounds	January 22, 2021	No response received	N/A
City of Portland Clerk Treasurer	January 26, 2021	January 26 & 27, 2021	C-30 to C-31

The Jay County School Corporation Transportation Department responded on August 26, 2020, stating that they did not receive the early coordination letter attachment. The attachment was resent on August 26, 2020; no additional response was received from the school with resource specific recommendations.

Resource specific recommendations are included in the applicable sections of this Categorical Exclusion (CE) document, and all applicable recommendations are included in the Environmental Commitments section of this CE document.

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SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

Federal Wild and Scenic Rivers
State Natural, Scenic or Recreational Rivers
Nationwide Rivers Inventory (NRI) listed
Outstanding Rivers List for Indiana
Navigable Waterways

Presence

X

Impacts

Yes	No
X	

Total stream(s) in project area: 200 Linear feet Total impacted stream(s): 52.75 permanent, 70 temporary Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
Salamonie River	R2UBH & R2UBHx	200	52.75 (permanent); 70 (temporary)	Flows northeast to southwest; tributary to the Wabash River & Water of the U.S. Refer to Appendix F-9, F-16, and F-17.

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, the aerial map of the project area (Appendix B-3), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E-8), there are seven (7) river and stream segments located within the 0.5-mile search radius. The Salamonie River flows northeast to southwest through the project area. The presence of this stream was confirmed by a site visit on August 19, 2020 by Green 3, LLC. No Federal, Wild and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways or National Rivers Inventory waterways are present in the project area.

A *Waters of the U.S. Determination/Wetland Delineation Report* was approved by the INDOT Ecology and Waterway Permitting Office (EWPO) on April 2, 2020 (Appendix F-1 to F-57). The site investigation for the report was completed by Metric Environmental, LLC on August 28, 2019. One (1) river, the Salamonie River, was identified within the project area. The Salamonie River is a perennial waterway and a tributary to the Wabash River, and is classified as a blue-line stream in the U.S Geological Survey (USGS) topographic map in Appendix B-2 and F-14. The Wabash River is located approximately 10 miles northeast of the project area. The dominant stream substrates are sand and silt. Pools were present and the only functional riffles observed are within the influence of the existing structure. The stream exhibits sparse amounts of instream cover which included undercut banks, overhanging vegetation, and logs and woody debris. No sinuosity was observed, and the water velocity was slow. Refer to site photos of the Salamonie River in Appendix B-5, B-6, and B-8, and Appendix F-25 to F-28. The Salamonie River is classified in this report as a Riverine Lower Perennial Unconsolidated Bottom Permanently Flooded (R2UBH) stream and a R2UBHx in the National Wetlands Inventory (NWI). No other streams, rivers, watercourses, or jurisdictional ditches were identified in the project area in the report.

This report also identified six (6) roadside ditches and four (4) drainage features in the study area. The roadside ditches and drainage features are located east and west of the existing structure on the north and south sides of the SR 26 roadway. Refer to Appendix F-17 to F-18 for locations. These features facilitate stormwater and/or roadside drainage and did not have an OHWM. These features were determined to be likely non-jurisdictional. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

Approximately 52.75 linear feet (0.007 acre) of Salamonie River will be permanently impacted by this project for removal of the existing bridge, the construction of Pier 2, and installation of riprap for scour protection around Pier 2. The project will also include construction of a cofferdam 70 feet in length, four (4) feet in width, and 0.88 feet in depth on the west side of Salamonie River that will result in temporary impacts to the waterway for approximately 60 days. The total area of temporary impact of the cofferdam is 70 linear feet (0.006 acre). Refer to Appendix B-23 for a plan sheet showing the cofferdam. Impacts to Salamonie River are unavoidable; if impacts were eliminated, the project would not be able to move forward, the purpose and need to improve the

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crossing over the waterway would not be met, and the existing bridge would continue to deteriorate, potentially resulting in eventual closure. Because stream impacts are less than 300 linear feet, no stream mitigation is expected to be required. A USACE Nationwide Permit (NWP), IDEM Rule 5 permit, and IDNR CIF permit are likely required for this project.

The USCG responded on September 3, 2020, stating that there is no sufficient factual support for concluding that the study area, at the project location, has current or historic navigation occurring in the waterway. Therefore, a USCG bridge permit or exemption is likely not required (Appendix C-12). Because the Salamonie River is not listed as a navigable waterway, a permit from IDNR is likely not required pursuant of the Indiana Navigable Waterways Act, Indiana Code (IC) 14-29-1.

The project falls under the "2013 USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana" (found at <https://www.in.gov/indot/2523.htm>). The USFWS responded on January 25, 2021 to the early coordination letter, noting that the Salamonie River provides quality, valuable habitat and requested that riparian trees be mitigated as close to the project impact as possible, preferably along the Salamonie River or a nearby tributary (Appendix C-29 to C-30).

The IDNR responded on September 24, 2020 to the early coordination letter with several recommendations to minimize impacts to streams (Appendix C-15 to C-17). Those include keeping cleared channel areas resulting in a flat bench area free of riprap for wildlife passage, and using smooth surfaced armoring material instead of riprap; restricting the size and placement of riprap; placing only enough riprap to provide stream bank toe protection, such as from the toe of bank up to the ordinary high water mark (OHWM); utilizing bioengineered bank stabilization methods instead of riprap; minimizing and containing within the project limits inchannel disturbance and the clearing of trees and brush; restricting work in the waterway from April 1 through June 30 without prior written approval of the Division of Fish and Wildlife; not excavating in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure; and avoiding the use of temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. The IDNR also recommended minimizing the movement of resuspended bottom sediment from the immediate project area; preventing debris from entering the waterway; using erosion control measures for preventing sediment from reaching the stream; and revegetating stream banks and disturbed areas. The IDEM automated early coordination response letter dated January 11, 2021 provided standard recommendations to obtain the necessary permits to work within waterways and to not remove overhanging trees near streams (Appendix C-21 to C-27). All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

Open Water Feature(s)

Reservoirs
Lakes
Farm Ponds
Retention/Detention Basin
Storm Water Management Facilities
Other: _____

Presence

X

Impacts

Yes	No
	X

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-3) there are six (6) open water features within the 0.5-mile search radius. There is one (1) open water feature, a lake, present adjacent to the project area, located approximately 0.02 mile north of the project area. The presence of this lake was confirmed by the site visit on August 19, 2020 by Green 3, LLC.

A *Waters of the U.S. Determination/Wetland Delineation Report* was approved by the INDOT EWPO on April 2, 2020 (Appendix F-1 to F-57). The site investigation for the report was completed by Metric Environmental, LLC on August 28, 2019. The report found one (1) open water feature in the report study area; this feature is the lake discussed above. Approximately 0.037 acre of the feature is located within the report study area. Refer to Appendix F-17 for the location of this lake. The USACE makes all final determinations regarding jurisdiction.

The project will acquire ROW from the south side of SR 26, with access proposed in the southeast quadrant and work occurring on the south side of the roadway and from the roadway. The lake identified and discussed above is outside of the project area of

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construction. Therefore, no impacts are expected. Refer to the plan sheet in Appendix B-15 for the location of this lake (labeled "pond" on the plan sheet) and the construction limits.

The IDEM automated early coordination response letter dated January 11, 2021 provided standard recommendations to obtain the necessary permits to work within waterways (Appendix C-21 to C-27). No other agencies responded with specific recommendations for impacts to open water features. All applicable recommendations are included in the Environmental Commitments section of this CE document.

Wetlands

Presence

☒ X

Impacts

Yes

☒ X

No

☐

Total wetland area: 0.133 Acre(s) Total wetland area impacted: 0.013 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland A	PF01A	0.128	0.013	Located in a drainage ditch south of SR 26 and east of the Salamonie River; likely a Water of the State (IDEM size-exempt; see discussion below)
Wetland B	PSS1A	0.005	0	Located in a drainage ditch on the north side of SR 26 and west of the Salamonie River; likely a Water of the State (IDEM incidental exempt; see discussion below)

Wetlands (Mark all that apply)

Wetland Determination
Wetland Delineation
USACE Isolated Waters Determination

Documentation

☒ X
☒ X
☐

ESD Approval Dates

July 9, 2020
July 9, 2020

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;
Substantially increased project costs;
Unique engineering, traffic, maintenance, or safety problems;
Substantial adverse social, economic, or environmental impacts, or
The project not meeting the identified needs.

☒ X
☐
☐
☐
☒ X

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Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E-3) there are seven (7) wetlands within the 0.5-mile search radius. Three (3) wetlands are mapped within the project area. That number was updated by the site visit on August 28, 2019, by Metric Environmental, LLC; only two (2) wetlands were identified during the site visit.

A *Waters of the U.S. Determination/Wetland Delineation Report* was approved by the INDOT EWPO on July 9, 2020 (Appendix F-1 to F-57). Two (2) wetlands were confirmed to be in the project area by the site visit on August 28, 2019, by Metric Environmental, LLC.

Wetland A is classified as a Palustrine, Forested, Broad-Leaved Deciduous, Temporarily Flooded (PF01A) wetland located in a drainage ditch within the floodplain of the Salamonie River. Wetland A is located on the south side of SR 26, east of the Salamonie River and project bridge. Wetland A likely receives stormwater drainage on a consistent basis during rain events. Wetland A is adjacent to road and forest and likely receives run-off from both sources. The east and west areas of Wetland A (0.113 acre and 0.015 acre, respectively) are separated by the existing 15-inch CMP under the agricultural access drive on the south side of the roadway (refer to the Waters Delineation Map in Appendix F-17; note: the report states that the CMP separating the east and west areas of Wetland A is a 16-inch CMP). These areas were determined to be one wetland due to proximity and topography, indicating that both areas share a hydrologic connection. Wetland A is dominated by reed canary grass (*Phalaris arundinacea*) and spotted touch-me-not (*Impatiens capensis*). The report concluded that Wetland A can support an average amount of wildlife or aquatic habitat and is therefore considered to be of average quality. The *Waters of the U.S. Determination/Wetland Delineation Report* stated that Wetland A does not directly abut a jurisdictional stream and should therefore be considered a Waters of the State (Appendix F-5).

Wetland B is classified as a Palustrine, Scrub-shrub, Broad-Leaved Deciduous, Temporarily Flooded (PSS1A) wetland located in a drainage ditch on the north side of SR 26 and west of the Salamonie River and project bridge. Wetland B is adjacent to road and residential property and likely receives run-off from both sources. Wetland B likely receives stormwater drainage on a consistent basis during rain events. Wetland B exhibits poor plant species diversity, contributing to a poor amount of wildlife or aquatic habitat, and therefore should be considered of poor quality. The *Waters of the U.S. Determination/Wetland Delineation Report* stated that Wetland B does not directly abut a jurisdictional stream and should therefore be considered Waters of the State (Appendix F-6). The USACE makes all final determinations regarding jurisdiction.

Approximately 0.013 acre of Wetland A, located east of the southeast quadrant of the bridge, will be permanently impacted by the project for construction access and grading. While approximately 0.11 acre of ROW acquisition from Wetland A is proposed, only a portion of it will be impacted. Refer to Appendix B-15 for the location of this impact. There is an unnamed cemetery and riparian forest in the northwest quadrant of the bridge, a trail system through a forested area and a special needs center (Jay-Randolph Developmental Services) and recycling facility in the southwest quadrant, and riparian forest as well as a steep grade away from the roadway directly adjacent to the bridge in the northeast quadrant. Due to overhead power lines on the south side of SR 26 and the project bridge, there are less trees directly adjacent to the project bridge and roadway. Construction access is planned to occur in the southeast quadrant of the project bridge to avoid the cemetery, trail, and special needs center and recycling facility, and to avoid the additional tree removal that would occur if the construction access were planned for the north side of SR 26 and the project bridge. Therefore, impacts to a portion of Wetland A are unavoidable to complete the project. No temporary impacts will occur to Wetland A, and no temporary or permanent impacts will occur to Wetland B.

During review of the project for required waterway permitting, an Approved Jurisdictional Determination (AJD) was obtained and the USACE declined to take jurisdiction over Wetlands A and B. IDEM determined that Wetland B was incidental exempt and Wetland A is size exempt. A USACE NWP is likely required for only impacts to the Salamonie River.

The IDNR responded on September 24, 2020, to the early coordination letter with recommendations to minimize impacts to wetlands, stating that IDEM and USACE may need to be contacted regarding impacts and that the project should not excavate or place fill in any riparian wetland (Appendix C-15 to C-17). The IDEM automated early coordination response letter dated January 11, 2021, provided standard recommendations to obtain the necessary permits to work within waterways (Appendix C-21 to C-27).

All applicable recommendations are included in the Environmental Commitments section of this CE document.

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	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 0.78 Acre(s) Total tree clearing: 0.12 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on August 19, 2020 by Green 3, LLC, and the aerial map of the project area (Appendix B-3), there is riparian forest, roadside vegetation, and lawn in the project area. The riparian forest surrounding the Salamonie River consists of black walnut (*Juglans nigra*), common hackberry (*Celtis occidentalis*), box elder (*Acer negundo*), white mulberry (*Morus alba*), giant goldenrod (*Solidago gigantea*), Indian hemp (*Apocynum cannabinum*), and reed canary grass (*Phalaris arundinacea*). Species along the SR 26 roadway outside of the riparian corridor include creeping thistle (*Cirsium arvense*) and field bindweed (*Convolvulus arvensis*). The project will require approximately 0.12 acre of small tree removal [saplings and trees under 10 inches in diameter at breast height (DBH)], the removal of up to six (6) trees over ten (10) inches in DBH, and the removal of approximately 0.75 acre of herbaceous vegetation, primarily on the east side of the Salamonie River. Total land disturbance is estimated to be 1.113 acres, including the removal of the bridge approaches and asphalt pavement on both sides of the project bridge. Avoidance would not allow the project to occur and the purpose and need to be met; the existing structure would continue to deteriorate, potentially leading to closure of the crossing over the Salamonie River. An IDEM Rule 5 permit is required due to land disturbance over one (1) acre. Tree mitigation due to tree removal in the floodway will be required as part of the IDNR CIF permit; coordination with IDNR is ongoing to determine appropriate tree mitigation requirements. The tree mitigation is expected to occur on-site.

The IDNR responded on September 24, 2020 with several recommendations regarding impacts to terrestrial habitat (Appendix C-15 to C-17). Recommendations include developing a mitigation plan for any unavoidable habitat impacts that will occur; mitigating impacts to non-wetland forest by planting five (5) trees at least two (2) inches in DBH for every tree over ten (10) inches removed; revegetating disturbed areas that will not be mowed with a mixture of grasses, sedges, and wildflowers native to eastern Indiana and only using turf-type grasses in regularly mowed areas; not cutting trees suitable for Indiana bat and northern long-eared bat roosting from April 1 to September 30; and seeding and protecting streambanks and slopes that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven/Leno-woven netting to minimize entrapment and snaring of small bodied wildlife. The USFWS responded on January 25, 2021, stating that the agency supports the woodland mitigation guidelines of the IDNR contained in their Information Bulletin #17, which gives mitigation ratios for different scenarios where trees are removed; the USFWS noted that the tree replacement requirement is not related to any possible mitigation needed for potential impacts to under the range-wide programmatic informal consultation process (Appendix C-29 to C-30). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
 Section 7 informal consultation completed (IPaC cannot be completed)
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Migratory Birds

Known usage or presence of birds (i.e. nests)
 State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E-11), completed by Green 3, LLC on March 19, 2020, the IDNR Jay Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR early coordination response letter dated September 24, 2020 (Appendix C-15), the Natural Heritage Program's Database has been checked and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C-35 to C-40). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat. No critical habitats were identified in the species list (Appendix C-40).

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. A bridge inspection occurred on August 19, 2020 and no bats or evidence of the presence of bats were found (Appendix C-53). An effect determination key was completed on January 15, 2021, and based on the responses provided, the project was found to "may affect, not likely to adversely affect" the Indiana bat and/or the NLEB (Appendix C-41 to C-52). INDOT reviewed and verified the effect finding on January 15, 2021 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the Environmental Commitments section of this document.

The IDNR responded to the early coordination letter on September 24, 2020 with recommendations regarding impacts to nesting birds and roosting bats (Appendix C-15 to C-17). Those include surveying the bridge for any bird nests prior to construction and between May 7 and September 7, the main nesting season for most bird species. If nests are found with eggs, chicks, or parents actively attending to the nest, then work should be put on hold until the nests complete their nesting cycle (to fledgling) or fall (by natural causes). The DNR also recommends bridge maintenance activities be restricted to November 1 and March 1 to avoid the summer roosting period for most bats in the central part of the state. The bridge must be inspected for the presence of bats; if there is evidence of active bat use, work must not occur until either the bats leave the structure for the season or a separate permit is issued to remove the bats. A formal survey to determine what species are present may be required and consultation with the state mammalogist or the USFWS is recommended when evidence of bat use of the structure has been observed.

INDOT Bridge No. 026-38-03430A (NBI No. 007040) did not show evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the August 19, 2020; however, due to the presence of Salamonie River and its forested riparian corridor, this structure provides suitable habitat for protected bird species. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure USP".

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

Project located within the Potential Karst Features Area of Indiana
Karst features identified within or adjacent to the project area
Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Study/Report reviewed by INDOT EWPO (if applicable): N/A

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Discuss if project is located in Potential Karst Features Area of Indiana and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Describe if any impacts will occur to any karst features. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Karst MOU and coordinated and reviewed by INDOT EWPO)

Based on a desktop review, the project is located outside of the designated karst region of Indiana as outlined in the October 13, 1993 Karst Memorandum of Understanding (MOU). According to the USGS topographic map of the project area (Appendix B-2) and the RFI report (Appendix E-3), there are no karst features identified within or adjacent to the project area. In the early coordination response on January 10, 2021, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features existing in the project area (Appendix C-18 to C-20). The IGWS indicated the project area has moderate liquefaction potential, is in a floodway, high potential for bedrock resources, and low potential for sand and gravel resources. No active or abandoned mineral resources extraction sites are documented in the area. These features will not be affected because the project will replace an existing structure along the same alignment with no extraction sites existing or planned within or adjacent to the project area. Response from IGWS has been communicated with the designer on January 10, 2021. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

Presence

X
X

Impacts

Yes	No
	X
	X

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Sole Source Aquifer

The project is located in Jay County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. A detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water Area

The IDEM Wellhead Proximity Determinator website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on August 11, 2020 by Green 3, LLC. This project is located within a wellhead protection area (WHPA). In an early coordination letter dated August 25, 2020, IDEM stated that the project is located in Portland Municipal Water Plant's WHPA and provided contact information for the Portland Municipal Water Plant (Appendix C-6). Therefore, an early coordination letter and project information was sent to the Portland Municipal Water Plant on August 25, 2020; no response was received. The features will not be affected because the project proposes replacement of an existing bridge along the same alignment and erosion control and spill prevention will comply with IDEM's WHPA Program. Avoidance alternatives would not be practical because the project replaces and existing structure in the WHPA and cannot be completed outside of the project area. The purpose and need of the project would not be met, and project bridge would continue to deteriorate, potentially leading to closure of the crossing over Salamonie River.

Water Wells

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on August 11, 2020 by Green 3, LLC. No wells are located near this project. Therefore, no impacts are expected.

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Urban Area Boundary

Based on a desktop review of <https://entapps.indot.in.gov/MS4/> by Green 3, LLC on January 12, 2020 for the RFI report, this project is located in an Urban Area Boundary (UAB). An early coordination letter was sent on August 25, 2020 to the City of Portland MS4 coordinator. The MS4 coordinator did not respond within the 30-day time frame. Based on the RFI report, an IDEM Rule 13 Permit has not been issued and no further coordination is necessary (Appendix E-3). An IDEM Rule 5 permit will be required, which will include erosion control measures that will control sediment runoff into waterways and public water resources.

Public Water System

Based on a desktop review, a site visit on a site visit on August 19, 2020 by Green 3, LLC, the aerial map of the project area (Appendix B-3), and review of the project plans in Appendix B-10 to B-22, no public water systems were identified within the project area. Therefore, no impacts are expected.

Floodplains

Project located within a regulated floodplain
Longitudinal encroachment
Transverse encroachment
Homes located in floodplain within 1000' up/downstream from project

Presence

X
X
X
X

Impacts

Yes	No
X	
X	
X	
	X

If applicable, indicate the Floodplain Level?

Level 1 ☐ Level 2 ☐ Level 3 ☐ Level 4 ☒ Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

Based on a desktop review of the IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) by SJCA Inc. (formerly Green 3, LLC) on April 2, 2021, and the RFI report, this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F-15). An early coordination letter was sent on August 25, 2020, to the local Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame. The IDNR responded to the early coordination letter on September 24, 2020, stating that the project will require formal approval of the agency for construction in floodway pursuant to the Flood Control Act (IC 14-28-1) unless the project qualifies for a bridge exemption (Appendix C-15). This project does not meet the bridge exemption criteria; therefore, an IDNR CIF permit will be required. This project qualifies as a Category 4 per the current INDOT CE Manual, which states:

Two (2) homes are located within the base floodplain within 1,000 feet upstream and zero (0) homes are located within the base floodplain within 1,000 feet downstream. The proposed structure will have an effective capacity such that backwater surface elevations are not expected to substantially increase. As a result, there will be no substantial adverse impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evacuation routes; therefore, it has been determined that this encroachment is not substantial. A hydraulic design study that addresses various structure size alternatives will be completed during the preliminary design phase. A summary of this study will be included with the Field Check Plans.

Farmland

Agricultural Lands
Prime Farmland (per NRCS)

Presence

X
X

Impacts

Yes	No
X	
X	

Total Points (from Section VII of CPA-106/AD-1006*)

90

**If 160 or greater, see CE Manual for guidance.*

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Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on August 19, 2020, by Green 3, LLC, and the aerial map of the project area (Appendix B-3), the project will convert approximately 0.001 acre of farmland as defined by the Farmland Protection Policy Act on the south side of SR 26 near the eastern project terminus. An early coordination letter was sent on August 25, 2020, to Natural Resources Conservation Services (NRCS), and a follow up email with the revised ROW estimate of 0.73 acre of permanent ROW and no temporary ROW acquisition was sent on April 5, 2021 (the August 25, 2020, early coordination letter stated that the project would require approximately 1.75 acres of permanent ROW and no temporary ROW). NRCS responded on April 9, 2021, stating the project will cause a conversion of prime farmland (Appendix C-13). Coordination with NRCS resulted in a score of 90 points on the NRCS AD 1006 Form (Appendix C-14). NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION D – CULTURAL RESOURCES

Minor Projects PA Category(ies) and Type(s) INDOT Approval Date(s) N/A
☐ ☐ ☒

Full 106 Effect Finding

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) ☐ Archaeology ☐ NRHP Bridge(s) ☒

Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination ☐
 800.11 Documentation ☐
 Historic Properties Report or Short Report ☐
 Archaeological Records Check and Assessment ☐
 Archaeological Phase Ia Survey Report ☐
 Archaeological Phase Ic Survey Report ☐
 Other: ☐

ESD Approval Date(s)

☐ 11/5/2020
☐ 3/3/2021
☐ 11/5/2020
☐
☐ 1/7/2021
☐
☐

SHPO Approval Date(s)

☐ 11/23/2020
☐ 3/25/2021
☐ 11/23/2020
☐
☐ 2/8/2021
☐
☐

Memorandum of Agreement (MOA) ☐

MOA Signature Dates (List all signatories)

☐ N/A

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Section 106 of the National Historic Preservation Act of 1966 (NHPA) requires that federal agencies identify and assess the effects of federal projects, programs, and actions on historic resources. This includes projects that are supported by federal funds. The Section 106 process was managed by SJCA Inc. (formerly Green 3, LLC), who is listed on the IDNR Department of Historic Preservation and Archaeology's Roster of Qualified Professionals.

As previously discussed, the project involves the replacement of a bridge that is eligible for listing on the NRHP under Criterion C as an example of an ISHC standard plan for a moderately-long span bridge and one of six (6) or fewer examples of this bridge type within the INDOT Greenfield District. Per the terms of the HBPA, the FHWA will satisfy its responsibilities involving "Select" and

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"Non-Select" bridges through the PDP of the HBPA (Stipulation III) to comply with Section 106 of the NHPA of 1966.

Area of Potential Effect (APE): According to 36 CFR 800.16(d), the APE is defined as "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in character or use of historic properties, if any such properties exist. The APE is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking..."

The Area of Potential Effects (APE) of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. The APE consists of agricultural fields, forested areas, and commercial and residential properties. The APE extends approximately 0.10 mile at its widest point to the west and extends 0.05 mile south at its longest point. Refer to Appendix D-12 for a description and Appendix D-19 for a map of the APE.

Coordination with Consulting Parties: On June 28, 2019, the following parties were sent an early coordination letter, project map, and an invitation to become a Consulting Party (see Appendix D-35 to D-37):

Consulting Party	Response
Indiana SHPO	July 23, 2019
Indiana Landmarks, Eastern Regional Office	February 13, 2020
Jay County Historian	No Response
Jay County Historical Society	March 31, 2020 (note: this entity did not respond to the June 28, 2019 invitation, but responded to the SHPO letter dated March 30, 2020 regarding the HBAA and was thereafter included as a participating consulting party)
Mayor of Portland	No Response
Portland Street Department	No Response
Portland Historic Preservation Commission	No Response
Jay County Commissioners	No Response
Jay County Highway Department	No Response
Dr. James Cooper	No Response
Historic Spans Task Force	No Response
Eastern Shawnee Tribe of Oklahoma	No Response
Miami Tribe of Oklahoma	July 24, 2019
Peoria Tribe of Indians of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	No Response
Delaware Tribe of Indians, Oklahoma	No Response
Forest County Potawatomi Community	No Response
Historicbridges.org	October 2, 2019 (note: this entity was not included in the June 28, 2019 invitation, but was provided information on historic bridge projects in Indiana in September 2019 as part of the new INDOT CRO procedures. This entity requested consulting party status to INDOT CRO on October 2, 2019)

Note: INDOT CRO is acting on behalf of FHWA. FHWA is the lead federal agency. The IDNR SHPO is an automatic consulting party.

The SHPO responded to the early coordination mailing on July 23, 2019 (Appendix D-38 to D-39), stating that the agency was not aware of anyone else who should be invited to become a consulting party for the purposes of the review of this project under Section 106, beyond whom INDOT has already invited. SHPO also indicated that for the purposes of IC 14-21-1-18 and 312 Indiana Administrative Code (IAC) 20-4, the agency has added the members of the Indiana Historic Review Board and additional, potentially interested parties to the list of parties the SHPO intends to copy with their comment letters. A list of these additional parties can be found in Appendix D-39.

In a letter dated July 24, 2019, the Miami Tribe of Oklahoma accepted the invitation to be a consulting party (Appendix D-40), stating that the Miami Tribe offers no objection to the project and that they were not aware of existing documentation linking a specific Miami cultural or historic site to the project site. However, the site is within the aboriginal homelands of the Miami Tribe and requested that immediate consultation be initiated if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of the project.

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In an email dated February 13, 2020, Indiana Landmarks – Eastern Regional Office accepted the invitation to be a consulting party (Appendix D-41). No other responses to the early coordination/invitation letter were received from consulting parties.

Archaeology: SJCA Inc.'s archaeologist conducted a Phase 1a archaeological reconnaissance investigation (Jackson, January 2021). The investigation identified one site which was recommended not eligible for the listing on the NRHP, and it was recommended no further archaeological work should be undertaken (Appendix D-68 to D-70). The archaeology report was approved by INDOT CRO and sent to consulting parties (SHPO and tribes only) on January 7, 2021 (Appendix D-58 to D-60). In a letter dated February 8, 2021, SHPO staff concurred with the findings of the archaeology report stating, "we concur with the opinion of the archaeologist, as expressed in the Phase 1a archaeological literature review and field reconnaissance survey report that Site 12-Ja-0700 (which was identified during these archaeological investigations) does not appear eligible for inclusion in the NRHP" (Appendix D-62 to D-64). SHPO also noted in this letter that it was their understanding that a cemetery development plan (CDP) was completed for the unnamed cemetery in the northwest quadrant of the bridge in 2009 and that an updated CDP will be developed. An updated CDP will be submitted before construction activities begin. This has been included in as a firm commitment in the Environmental Commitments section of this document. No other responses were received regarding the archaeological report.

Historic Properties: In accordance with the HBPA, a Historic Bridge Alternatives Analysis (HBAA) was completed. The HBAA was approved by INDOT CRO and provided to consulting parties on March 3, 2020 for review and comment (Appendix D-45 to D-47). As discussed in the Alternatives sections above, six (6) alternatives, including the preferred and Do Nothing alternatives, were examined. The SHPO responded on March 30, 2020 after their review of the HBAA, stating that Alternative E, relocation of the historic bridge and new bridge construction, is prudent only if a responsible party steps forward to fund the relocation, rehabilitation, and maintenance of the bridge, and if a responsible party does not step forward during the bridge marketing period, the SHPO was "satisfied that Alternative F: Replacement-Demolition of Historic Bridge and New Bridge Construction is the only remaining alternative that is feasible and prudent" (Appendix D-48 to D-50). SHPO also asked in their March 30, 2020 response if elements of the bridge could be stored regardless of whether a responsible party stepped forward to take responsibility of the bridge. SHPO also requested that the bridge be photographically documented prior to commencement of the project in accordance with the HBPA in keeping with the applicable standards of the "Indiana DNR-Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards". INDOT CRO's responses to SHPO's comments were included in the November 5, 2020 report distribution letter for the historic property report (HPR).

On March 31, 2020 the Jay County Historical Society responded and provided information regarding the unnamed cemetery in the northeast quadrant of the bridge but did not provide any comments regarding the HBAA (Appendix D-51). No other responses to the HBAA were received from consulting parties. The HBAA can be reviewed in Appendix J-2 to J-142.

An HPR was completed for this project (Wood, November 2020). INDOT CRO approved the HPR for distribution to SHPO and consulting parties on November 5, 2020. The HPR was made available to SHPO and Consulting Parties for review on November 5, 2020 (Appendix D-52 and D-53). The HPR distribution letter included responses to SHPO's comments in their March 30, 2020 response letter to the HBAA regarding photo documentation of the project bridge and storage of bridge elements. INDOT completed the photographic documentation in December 2020 in accordance with the HBPA and coordinated with the Jay County Historical Society to store the photos and make them available to the public. INDOT responded to SHPO's question regarding storage of bridge elements, stating the agency did not intend to salvage and store any elements of the bridge unless a responsible party comes forward (Appendix D-16).

The HPR found no above-ground properties in the APE that are listed in the NRHP and recommended the project bridge, Bridge No. 041-82-03286HSBL (NBI No. 014310), remain eligible for listing in the NRHP. Excerpts of the HPR is in Appendix D-66 to D-67. The HPR noted that INDOT CRO staff conducted an aboveground resources investigation for the SR 26 pavement replacement project under Des. No. 0100715 in 2008, which recommended one property, Bridge No. 026-38-03430A (NBI No. 007040), as eligible for the NRHP under Criterion C (Appendix D-14).

On November 23, 2020, SHPO staff responded to the HPR, concurring with the conclusions of the HPR that the bridge in this project (Bridge 026-38-03430A, NBI 007040) is the only aboveground historic property eligible for inclusion in the NRHP within the APE (Appendix D-54 to D-55). No other responses to the HPR were received from consulting parties.

Documentation Findings: Per CFR Section 800.4(d)(1) and the HBPA, because there are no historic properties present that are listed or eligible for the NRHP besides the project bridge, a finding of "no historic properties affected" was issued on behalf of the FHWA by INDOT CRO on March 3, 2021 (Appendix D-7 to D-10).

SHPO responded on March 25, 2021 to the effect finding documentation, stating that the agency concurred with the "no historic

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properties affected” finding (Appendix D-4 to D-6). No other consulting parties responded to the effect finding mailing within the 30-day comment period, which ended on April 2, 2021.

It should be noted that per the permanent rule issued by the IDNR effective August 14, 2013 (312 IAC 20-4-11.5), INDOT has requested that this project be subjected to "dual review"; that is, reviewed by the DHPA simultaneously under 54 United States Code (U.S.C.) 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law concerning alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process INDOT anticipates that the Division Director will issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. A request for the letter of clearance will be made upon submission of the CDP to SHPO. This is included as a firm commitment in Environmental Commitments section of this document.

Public Involvement: Per the marketing requirements of the HBP, on March 9, 2020, two public notices were published in the *Indianapolis Star* and *The Commercial Review* offering the bridge to interested responsible parties for the rehabilitation and reuse, the storage and future reuse, or salvage elements of the bridge. The bridge was placed on the INDOT Historic Bridges Marketing Program website (<https://www.in.gov/indot/2532.htm>) on February 12, 2020, and marketing signs were posted on the west and east sides of the bridge. The bridge is currently listed as “pending” on the INDOT Historic Bridge Marketing website. No sponsors to take responsibility of the relocation and preservation of the bridge have been identified as of the date of this CE document, and the bridge is still currently listed on the INDOT bridge marketing website as “pending.” Refer to Appendix D-71 to D-76 for all marketing documents.

A notice informing the public of the finding and opportunity to comment on the “no historic properties affected” finding was published in the *Commercial Review* on March 6, 2021 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). The published public comment period closed on April 7, 2021. The text of the public notice and the affidavit of publication appear in Appendix D-1 to D-3. No comments from the public were received during the published comment period ending on April 7, 2021.

Pursuant to the Historic Bridge Programmatic Agreement (PA), a public hearing is required. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. All originally invited consulting parties will receive the hearing notice. This document will be revised after the public involvement requirements are fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

[illegible]

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Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B-3), the HPR (Appendix D-66 to D-67), and the RFI report (Appendix E-2), there are nine (9) potential 4(f) resources located within 0.5-mile search radius:

- Two (2) recreational facilities, East Elementary School located approximately 315 feet southwest of the western project terminus and Portland Waterpark, which is located 0.35 mile southwest of the western project terminus
- Six (6) trails, with the closest being approximately 215 linear feet south of the project area on the west side of the Salamonie River.
- One (1) NRHP eligible property, Bridge No. 026-38-03430A (NBI No. 007040), the bridge in this project

Note: the RFI report states that East Elementary School is located adjacent to the project area; based on current project limits, the school is approximately 315 feet southwest of the western project terminus and therefore is in the vicinity of the project but not adjacent to the project area. Refer to the plan sheet in Appendix B-15 and B-16 for the current project limits and property owner information. The western project terminus is located east of Jack Street; East Elementary School is located west of Jack Street.

According to additional research using the Jay County Geographic Information System (GIS) mapping tool (<http://www.jaycounty.net/plugins/content/content.php?content.14>) and a site visit on August 19, 2020, there is a recreational trail system not identified in the RFI report located approximately 85 feet southwest of the project bridge approach on property owned by the Jay-Randolph Developmental Services and recycling facility. Refer to the site photo in Appendix B-9 and the plan sheet in Appendix B-15 for the trail system's location. Because the trail system is not located on publicly owned property, it is not considered a Section 4(f) resource. This was confirmed by INDOT Greenfield District on September 1, 2020 (Appendix C-10). Based on current design, no impacts will occur to this unmapped trail system. Also, the RFI report states that the closest trail is located adjacent to the project area; additional research indicated that the trails named "Additional Nature Trails Completed" mapped on the east side of the Salamonie River and south of the eastern project terminus. Research found that these trails are mapped incorrectly and are actually located in Hudson Family Park, approximately 0.51 mile from the project area. Research using the Jay County GIS mapping tool and review of the plan sheet in Appendix B-16 indicated that the property on the south side of SR 26, east of the Salamonie River, is privately owned and not a public park or recreational area. Refer to Appendix E-7 for the locations of mapped and unmapped trails within 0.5 mile of the project area.

Early coordination letters were sent to East Elementary School and the City of Portland Streets and Parks Superintendent on August 25, 2020. No responses were received. Because all trails mapped south of the project area are outside of the project construction limits and no work will impact the use of any trails, no impacts are expected. Because East Elementary School is located outside of the project area, no impacts to the school as a recreational facility are expected.

The project bridge, Bridge No. 026-38-03430A (NBI No. 007040), is afforded protection under Section 4(f) as a historic site that is eligible for listing on the NRHP. The Section 4(f) statute places restrictions on the use of land from historic sites for highway improvements but makes no mention of historic bridges or highways that are already serving as transportation facilities. The HPR (Wood, November 2020) did not identify any other historic resources afforded protection under Section 4(f) in the project APE besides the project bridge.

The proposed bridge project qualifies for the programmatic Section 4(f) evaluation and approval for FHWA projects that necessitate the use of a historic bridge when the project meets the following criteria:

1. The bridge is to be replaced or rehabilitated with Federal funds.
2. The project will require the use of a historic bridge structure which is on or is eligible for listing on the NRHP.
3. The bridge is not a National Historic Landmark.
4. The FHWA Division Administrator determines that the facts of the project match those set forth by the investigation of the appropriate Alternatives, Findings, and Mitigation.
5. Agreement among the FHWA, the SHPO, and the ACHP has been reached through procedures pursuant to Section 106 of the NHPA.

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This historic bridge project meets these criteria. To apply the Historic Bridge Programmatic Section 4(f) Evaluation, three alternatives that avoid any use of the historic bridge must be examined: do nothing, rehabilitate the historic bridge without affecting the historic integrity of the structure, and build a new structure at a different location without affecting the historic integrity of the historic bridge. The HBAA evaluated these three (3) required alternatives. The Indiana HBPA requires a more extensive alternatives analysis evaluating additional alternatives and that guidance was incorporated into this project.

The alternatives described in this document are based on the guidance from INDOT CRO for writing a historic bridge Section 4(f) alternatives analysis, which can be found in Appendix J-1 to J-142. Per the guidance, project alternatives must be analyzed in consecutive order until a feasible and prudent alternative has been determined which also results in the least amount of harm to the protected resource. A feasible alternative is one that is possible to engineer, design, and build, and a prudent alternative is one that does not present significantly unique or unusual factors (e.g. cost; social, economic, or environmental impacts; community disruption). Once a feasible and prudent alternative has been determined, the remaining alternatives do not need to be analyzed. The following alternatives were previously discussed in detail in the "Alternatives" sections of this CE document:

- Alternative A: Do Nothing
- Alternative B: Rehabilitation of Existing Structure for Continued Vehicular Use (two-lane option) Meeting Secretary of Interior's Standards for Rehabilitation
- Alternative C: Rehabilitation of Existing Structure for Continued Vehicular Use (one-way pair option) Meeting Secretary of Interior's Standards for Rehabilitation with Construction of New One-Way Structure with Construction of New One-Way Structure
- Alternative D: Bypass (non-vehicular use) / Build New Structure without Affecting the Historic Integrity
- Alternative E: Relocation of Historic Bridge and New Bridge Construction
- Alternative F: Replacement – Demolition of Historic Bridge and New Bridge Construction

Alternatives E and F meet the project's purpose and need to improve the SR 26 crossing over the Salamonie River. Alternative E is feasible, meeting all current INDOT design standards. For a "Non-Select" bridge, Alternative E is only prudent if a responsible party other than the owner (INDOT) comes forward to fund the relocation, rehabilitation, and maintenance of the bridge. The bridge was placed on the INDOT Historic Bridges Marketing Program website (<https://www.in.gov/indot/2532.htm>) on February 12, 2020, and marketing signs were posted on the west and east sides of the bridge. The bridge is currently listed as "pending" on the INDOT Historic Bridge Marketing website. No sponsors to take responsibility of the relocation and preservation of the bridge have been identified as of the date of this CE document, and the bridge is still currently listed on the INDOT bridge marketing website as "pending." Alternative F is also feasible and meets all current INDOT design standards; if no responsible party other than the owner comes forward to fund the relocation, rehabilitation, and maintenance of the bridge, this alternative is prudent.

Pursuant to the Programmatic Section 4(f) Evaluation and Approval for FHWA projects that necessitate the use of historic bridges, the preliminary preferred alternatives (Alternatives E and F) will result in a use of the historic bridge. The FHWA signature of this Level 4 CE document will serve as the FHWA concurrence of this Programmatic Section 4(f) evaluation for Bridge No. 026-38-03430A (NBI No. 007040).

Section 6(f) Involvement

Section 6(f) Property

Presence

Use

Yes

No

☐☐☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of the list of 6(f) properties on the INDOT ESD website revealed a total of two (2) properties in Jay County (Appendix I-1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

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SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?
Is the project located in an MPO Area?
Is the project in an air quality non-attainment or maintenance area?

Yes	No
X	
	X
	X

If Yes, then:

Is the project in the most current MPO TIP?
Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?
Is a hot spot analysis required (CO/PM)?

Location in STIP:

2020-2024 STIP, page 239 (Appendix H-1)
2018-2021 STIP, page 308 (Appendix H-2)

Name of MPO (if applicable):

N/A

Location in TIP (if applicable):

N/A

Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

This project is included in the Fiscal Year (FY) 2020-2024 STIP (approved July 2, 2019) for ROW and construction. The project is also listed in the 2018-2021 STIP (approved July 3, 2017) for engineering. Refer to Appendix H-1 and H-2.

This project is located in Jay County, which is currently in attainment for all criteria pollutants according to IDEM's current and historical list of nonattainment areas by county, which can be found at: <https://www.in.gov/idem/airquality/2339.htm>. Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE

Noise

Yes No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

	X
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Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

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SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?
Will the proposed action result in substantial impacts to community cohesion?
Will the proposed action result in substantial impacts to local tax base or property values?
Will construction activities impact community events (festivals, fairs, etc.)?
Does the community have an approved transition plan?
If No, are steps being made to advance the community's transition plan?
Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project complies with the Jay County Economic Development Corporation's 2018 Strategic Plan goal to strengthen and improve physical infrastructure and address infrastructure needs (<http://www.jaycountyeconomicdevelopment.org/wp-content/uploads/2019/01/2018-Strategic-Plan.pdf>). The project also complies with the City of Portland Urban Revitalization Plan (2016) to promote multi-modal forms of transportation (https://thecityofportland.net/e107_images/file/20_portland_revitalization_plan_2016_small.pdf). There are currently no pedestrian facilities in the project area; the project will construct a new 6-foot, 3-inch-wide concrete sidewalk on the south side of SR 26 from the driveway entrance to the Jay-Randolph Developmental Services and recycling facility to the west end of the new bridge structure. The west end of the new sidewalk will end with an ADA compliant transition at the end of the new guardrail. A 6-foot, 10-inch-wide concrete sidewalk will be constructed on the new bridge structure only. The new sidewalks will provide opportunities for connection to the existing and planned trails south of the project (refer to the RFI map in Appendix E-7). The new sidewalk will be constructed in compliance with the City of Portland ADA Plan, which was passed by resolution on January 16, 2017 (available from the City of Portland by request; refer to Appendix C-31). The project also complies with the 2017 Jay County Transition Plan for Public Rights-of-Way, which includes several measures to ensure ADA compliance (www.jaycounty.net/images/file/78_jay_county_ada_transition_plan.pdf).

A search of events on the Jay County Visitors and Tourism Bureau website (<https://www.visitjaycounty.com/events>) found several events in the county and in the City of Portland area. Several events are held at the Jay County Fairgrounds, located about 0.51 mile north of the project bridge on Division Road. The Tri-State Gas Engine and Tractor Association, Inc. holds an annual tractor show in August and a swap meet in May and October, with thousands in attendance (<https://tristategasenginetractor.com/index.php>). Therefore, the early coordination letter was sent to the Tri-State Gas Engine and Tractor Association, Inc. organizers, the Jay County Fairgrounds, and the Jay County Visitors and Tourism Bureau on January 22, 2021 (Appendix C-5). A follow-up email was sent on February 20, 2021. A representative of the Tri-State Tractor Show responded on February 22, 2021 (Appendix C-33), stating that the organization did not have any concerns since construction is proposed from October to April. No other responses from these entities were received. If the construction dates are modified so that impacts to the Tri-State Gas Engine and Tractor Association events may occur, coordination with the Tri-State Gas Engine and Tractor Association will occur in accordance with the Environmental Commitments at the end of this document.

Travel to and from events in the City of Portland may be minimally impacted by the project. Since an official detour with clearly marked signage will be used during construction, impacts to community cohesion and local events will be minimized. It was concluded that the project will not substantially impact community cohesion or adversely impact local community events.

This project will involve replacement of an existing bridge structure along the same alignment and will not substantially impact the tax base or property values. Approximately 0.731 acre of permanent ROW acquisition is proposed from only the south side of the SR 26 roadway and bridge from private properties and include the riparian corridor of the Salamonie River. The project will not have a substantial impact to local tax bases and property values. This project will have a net positive effect on the community because it will ensure an improved crossing over the Salamonie River.

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Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B-3), the project plans in Appendix B-10 to B-22, and the RFI report (Appendix E), there are five (5) public facilities within the 0.5-mile radius of the project. There are no public transportation facilities within or adjacent to the project area.

One (1) religious facility is located approximately 0.42 mile from the project area. No impacts are expected.

There are two (2) schools, with the closest being East Elementary School located approximately 315 linear feet southwest of the western project terminus. The MOT plan involves closure of the project bridge and a detour, which will result in temporary impacts to school bus routes. The Jay County School Corporation and East Elementary School were sent early coordination letters on August 25, 2020. No response was received from East Elementary School. The Jay County School Corporation responded on August 26, 2020 requesting that the early coordination project information attachment be resent but did not provide any additional responses or comments (Appendix C-8).

There are no airports within the 0.5-mile search radius; however, there is one (1) airport located approximately 1.69 miles northwest of the project area. An early coordination letter was sent to INDOT Aviation was sent on August 25, 2020. The agency responded on August 26, 2020 stating that if any object, obstruction, or equipment will exceed 95 feet in height, further coordination will be required due to the proximity of Portland Municipal Airport and the need for any obstructions within five (5) miles to meet a 100:1 glideslope to the nearest runway (Appendix C-7). Construction equipment such as cranes used for this project will likely exceed 95 feet in height. This information was forwarded to INDOT Aviation on May 17, 2021; the agency responded on May 18, 2021 (Appendix C-34), stating that the project should be submitted to the FAA for airspacing. An airspace permit determination is in process to the FAA for review; If the FAA finds that the equipment will interfere with airspace, a formal FAA permit will be obtained in a timely manner and included in the project contract documents. This has been included in the Environmental Commitments at the end of this document.

There is one (1) cemetery mapped adjacent to the project area in the northwest quadrant of the bridge. This cemetery does not have a parking area or driveway entrance. Based on coordination with INDOT CRO, SHPO, and consulting parties, due to work within 100 feet of the cemetery, an updated CDP will be completed prior to construction in accordance with IC 14-21-1-26.5. Refer to Appendix D-9.

As discussed previously, there are currently no pedestrian facilities within the project area. The existing trail system, also a pedestrian and bicycle facility, is located approximately 85 feet south of the project area and will not be impacted by the project. Refer to Appendix B-15 for the location of this trail system.

According to online research, the Jay-Randolph Developmental Services is a private, nonprofit organization located near the southwest quadrant of the project bridge that provides services for people with special needs. Based on a site visit by Green 3, LLC on August 19, 2020, there is a recycling facility attached to the Jay-Randolph Development Services building. Access will be maintained throughout construction. Therefore, no impacts are anticipated.

There are several utilities located within the project area, including overhead power, communication, and telephone lines and buried fiber optic lines. Refer to Appendix B-10 to B-22 for project plans showing the locations of utilities. Relocation of these utilities is anticipated; coordination with utility owners has been initiated and will continue throughout project development and construction.

The bridge closure and detour will temporarily impact emergency services. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

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Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high and adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require approximately 0.73 acre of permanent right-of-way and no relocations. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Jay County. The western portion of the project is within the corporate limits of the City of Portland, Indiana, and the bridge crossing and eastern portion of the project is outside of the corporate limits of the City of Portland. The community that overlaps the project area is called the affected community (AC). In this project, the ACs are Census Tract 9629 and Census Tract 9630 in Jay County. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2018 American Community Survey (ACS) 5-year estimates was obtained from the US Census Bureau Website <https://factfinder.census.gov/> on December 13, 2020 by SJCA Inc. The data collected for minority and low-income populations within the AC are summarized in the below table:

Table: Minority and Low-Income Data (Source Data and Year)			
	COC – Jay Co.	AC-1 - Census Tract 9629, Jay County, Indiana	AC-2 - Census Tract 9630, Jay County, Indiana
Percent Minority	5%	2.9%	11.5%
125% of COC	6.2%	AC < 125% COC	AC > 125% COC
EJ Population of Concern		No	Yes
Percent Low-Income	16.9%	11.7%	13.2%
125% of COC	21.1 %	AC < 125% COC	AC < 125% COC
EJ Population of Concern		No	No

AC-1, Census Tract 9629, has a percent minority of 2.9% which is below 50% and is below the 125% COC threshold. AC-2, Census Tract 9630, has a percent minority of 11.5% which is below 50% but is above the 125% COC. Therefore, AC-2 is a minority population of EJ concern.

AC-1, Census Tract 9629, has a percent low-income of 11.7% which is below 50% and is below the 125% COC threshold. AC-2, Census Tract 9630, has a percent low-income of 13.2% which is below 50% and is below the 125% COC threshold. Therefore, both AC's do not contain low-income populations of EJ concern.

The project will provide community-wide positive impacts in the form of an improved crossing over Salamonie River for all travelers regardless of income or ethnicity. Right-of-way acquisition will occur along the roadway and riparian corridor of the river, without relocation of residences or businesses. The detour route will impact all travelers regardless of income or ethnicity and will not impact EJ populations more than any other population. The EJ analysis conducted for this project was forwarded to INDOT ESD on December 22, 2020, who responded on December 31, 2020, stating that "with the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low-income populations of EJ concern relative to non-EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a". No further EJ Analysis is required. All EJ Analysis documentation is in Appendix I.

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Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
Is a BIS or CSRS required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)
Phase I Environmental Site Assessment (Phase I ESA)
Phase II Environmental Site Assessment (Phase II ESA)
Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

Date RFI concurrence by INDOT SAM (if applicable): 3/19/2020 (no updates required per INDOT SAM; see Appendix E-12).

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of GIS and available public records, a RFI was completed by Green 3, LLC and concurred by INDOT Site Assessment & Management (SAM) on March 19, 2020 (Appendix E-5). Eight (8) hazmat sites are located within 0.5 mile of the project area:

- One (1) underground storage tank (UST) located approximately 0.16 mile west of the project
- One (1) leaking underground storage tank (LUST) located approximately 0.49 mile northwest of the project
- One (1) brownfield located approximately 0.45 mile west of the project
- Two (2) National Pollutant Discharge and Elimination System (NPDES) facilities; the closest of these is located approximately 0.35 mile west of the project
- Three (3) NPDES pipe locations; the closest of these is located approximately 0.26 mile southwest of the project

None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

Due to the RFI being approved over one (1) year from the date of this CE document an error was found regarding the location of mapped trails, INDOT SAM was contacted to determine if updates to the report are needed. INDOT SAM responded on April 7, 2021 stating that an addendum to the RFI report was not needed (Appendix E-12).

Indiana Department of Transportation

County Jay Route SR 26 over Salamonie River Des. No. 1600828

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Other

X

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Isolated Wetlands
Rule 5
Other

X

IN Department of Natural Resources

Construction in a Floodway
Navigable Waterway Permit
Other

X

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

X

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

At this time, a USACE NWP is required due to impacts to Waters of the U.S. (Salamonie River). An IDEM Rule 5 permit is required due to the disturbance of more than one (1) acre of land. The IDNR responded on September 24, 2020, stating that the project will require the formal approval of the agency for construction in a floodway pursuant of the Flood Control Act (IC 14-28-1) unless it qualifies for a bridge exemption (Appendix C-15). An IDNR CIF permit is required due to work within the floodway. Due to the drainage area of Salamonie River having a drainage area of greater than one (1) square mile, the project does not fall under any bridge exemptions. An updated CDP shall be completed prior to construction in accordance with IC 14-21-1-26.5.

An FAA permit for airspacing may be required due to the use of construction equipment over 95 feet in height. An airspace permit determination is in process to the FAA for review; If the FAA finds that the equipment will interfere with airspace, a formal FAA permit will be obtained in a timely manner and included in the project contract documents.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

Indiana Department of Transportation

County Jay Route SR 26 over Salamonie River Des. No. 1600828

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

- Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)
- If the scope of work or permanent or temporary right-of-way amounts change, INDOT ESD and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD)
- It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction activity that would block or limit access. (INDOT ESD)
- USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after August 19, 2022, an inspection of the structure by a qualified individual, must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT ESD)
- A hydraulic design study that addresses various structure size alternatives will be completed during the preliminary design phase. (INDOT ESD)
- The project will comply with IDEM's WHPA Program to ensure contaminants do not negatively impact the WHPA in the project area. (INDOT ESD)
- The portion of Wetland A to be avoided by construction will be labeled on the plans as "Do Not Disturb". (INDOT ESD)
- Wetland B will be included on the plans and labeled as "Do Not Disturb". Wetland B shall not be impacted by construction. (INDOT ESD)
- Before the commencement of construction activities, INDOT will obtain a Director's Letter of Clearance for the project, indicating compliance with Indiana Code 14-21-1-18. (INDOT CRO, SHPO)
- Pursuant of 2013 (312 IAC 20-4-11.5), INDOT CRO has requested that this project be subjected to "dual review" and reviewed by the DHPA simultaneously under 54 U.S.C. 306108 and IC 14-21-1-18. At the conclusion of the review process INDOT anticipates that the Division Director will issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18. A request for the letter of clearance will be made upon submission of the CDP to SHPO. (INDOT CRO, SHPO)
- An updated CDP shall be completed prior to construction in accordance with IC 14-21-1-26.5. (INDOT CRO, SHPO)
- Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
- Tree Removal AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS, IDNR-DFW)
- Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g. install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits. (USFWS)
- Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roost that are still suitable for roosting, or trees within 0.25 mile of roosts, or documented foraging habitat any time of year. (USFWS)
- General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- If any object, obstruction, or equipment will exceed 95 feet in height, further coordination will be required due to the proximity of Portland Municipal Airport and the need for any obstructions within five (5) miles to meet a 100:1 glideslope to the nearest runway. An airspace permit determination is in process to the FAA for review; If the FAA finds that the equipment will interfere with airspace, a formal FAA permit will be obtained in a timely manner and included in the project contract documents. (INDOT Aviation)
- INDOT Bridge No. 026-38-03430A (NBI No. 007040) did not show evidence of use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA) during the August 19, 2020; however, due to the presence of Salamonie River and its forested riparian corridor, this structure provides suitable habitat for protected bird species. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the

Indiana Department of Transportation

County Jay Route SR 26 over Salamonie River Des. No. 1600828

nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the “Potential Migratory Bird on Structure USP”. (INDOT)

19. Tree mitigation due to tree removal in the floodway will be required as part of the IDNR CIF permit. It is anticipated that this mitigation will occur on-site. However, if off-site mitigation is determined to be required, a separate environmental document will be required. (INDOT)
20. Survey the bridge for any bird nests prior to construction and between May 7 and September 7, the main nesting season for most bird species. If nests are found with eggs, chicks, or parents actively attending to the nest, then work should be put on hold until the nests complete their nesting cycle (to fledgling) or fall (by natural causes). (IDNR-DFW)
21. The IDNR also recommends bridge maintenance activities be restricted to November 1 and March 1 to avoid the summer roosting period for most bats in the central part of the state. (IDNR-DFW)

For Further Consideration:

22. The USFWS requests that the loss of riparian trees be mitigated as close to the project impact site as possible, preferably along the Salamonie River or nearby tributary. (USFWS)
23. The USFWS supports the woodland mitigation guidelines of the IDNR contained in their Information Bulletin #17, which gives mitigation ratios for different scenarios where trees are removed, noting that the tree replacement requirement is not related to any possible mitigation needed for potential impacts to under the range-wide programmatic informal consultation process. The mitigation ratio for non-wetland floodway forest losses of more than 1 acre is to be 2:1 (2 acres replanted for every acre destroyed), planted as close to the impact site as possible; loss of less than an acre is to be mitigated at a 1:1 ratio or 5 trees for each lost tree of 10 inches DBH or greater. If any of the woodland that would be removed is forested wetland, the mitigation ratio is 4:1. (USFWS)
24. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
25. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure. (USFWS)
26. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
27. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30); except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below the OHWM during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
28. Keep cleared channel areas resulting in a flat bench area free of riprap for wildlife passage and use smooth surfaced armoring material instead of riprap. (IDNR-DFW)
29. Place only enough riprap to provide stream bank toe protection, such as from the toe of bank up to the OHWM. (IDNR)
30. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR-DFW)
31. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR-DFW)
32. Avoid the use of temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR)
33. The IDNR recommends a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The IDNR's Habitat Mitigation guidelines (and plant lists) can be found online at: <http://iac.iga.in.gov/iac/20200527-IR-3122000284NRA.xml.pdf>. (IDNR-DFW)

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Appendix F – Water Resources

7/9/2020 *Waters of the U.S. Determination / Wetland Delineation Report* F1

Appendix G – Public Involvement (To be updated once public involvement activities are complete)

Notice of Entry Letters G1

Appendix H – Air Quality

Indiana STIP FY 2020-2024 Documentation H1

Indiana STIP FY 2018-2021 Documentation H2

Appendix I – Additional Studies and Information

Land & Water Conservation Fund (LWCF) properties in Jay Co. I1

EJ Analysis & US Census Data I2

Appendix J – Historic Bridge Alternatives Analysis

February 2020 Historic Bridge Alternatives Analysis by USI Consultants, Inc. J1

Appendix A

Threshold Table

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁶)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level <ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

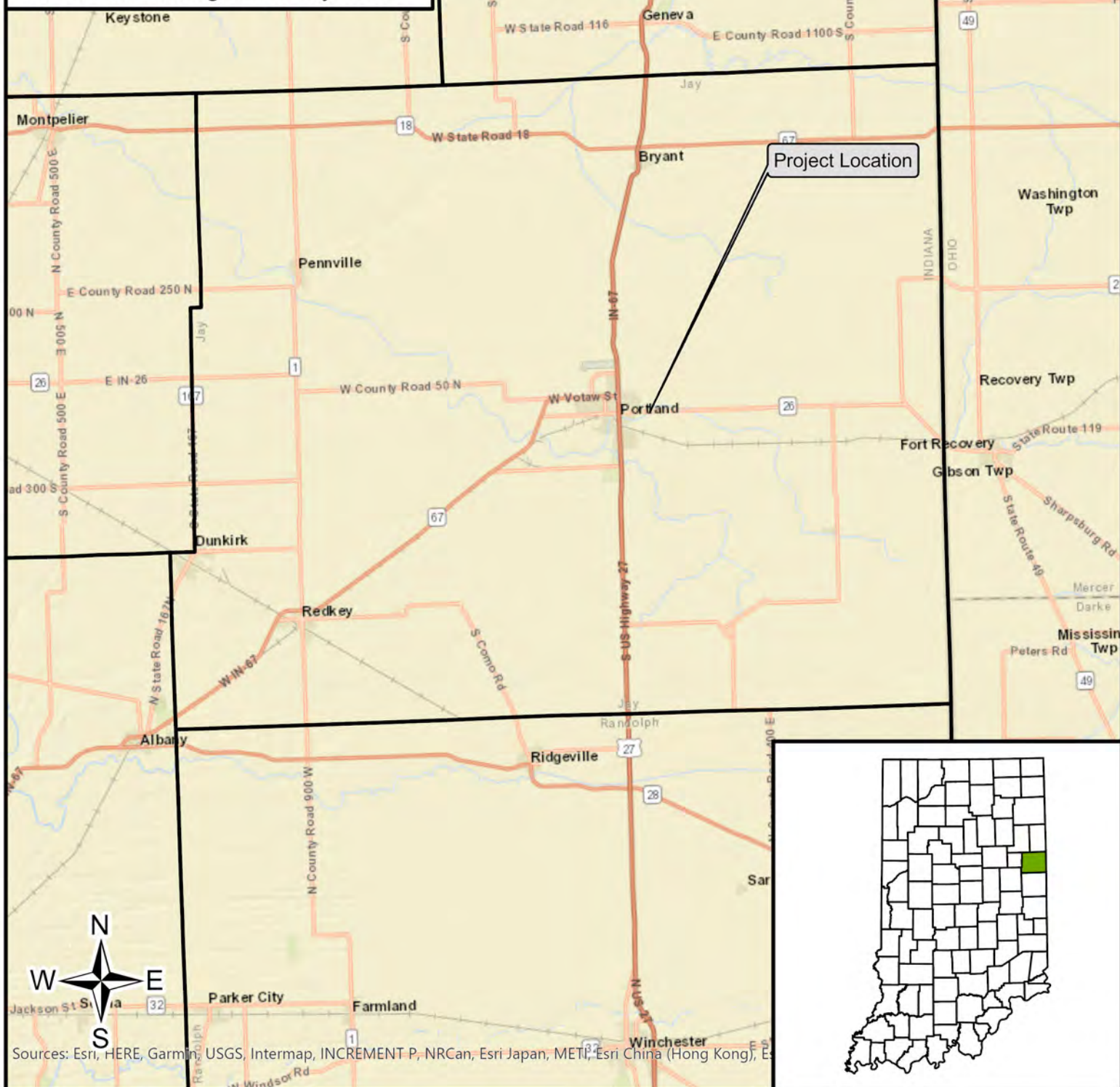
* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics

Project Location Map (1:250,000)
 Bridge Rehabilitation Project
 SR 26 over Salamonie River
 Des. No. 1600828
 Jay County, Indiana
 Source: US Geological Survey PLSS



0 2.5 5
 Miles

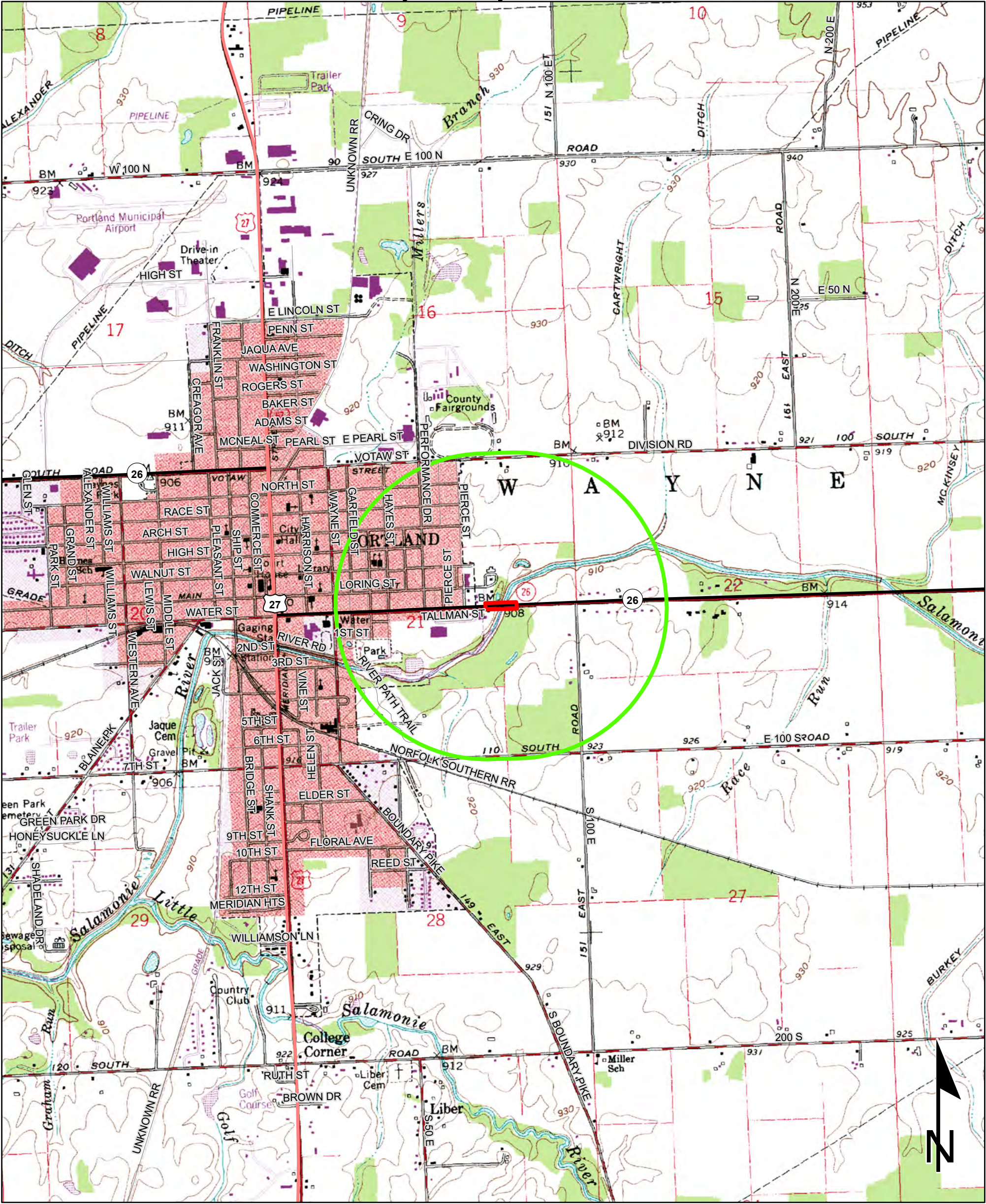
County Boundary
 Project County

green • 3



7/9/2020

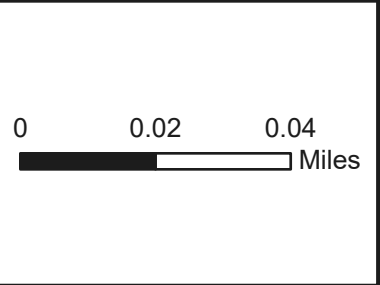
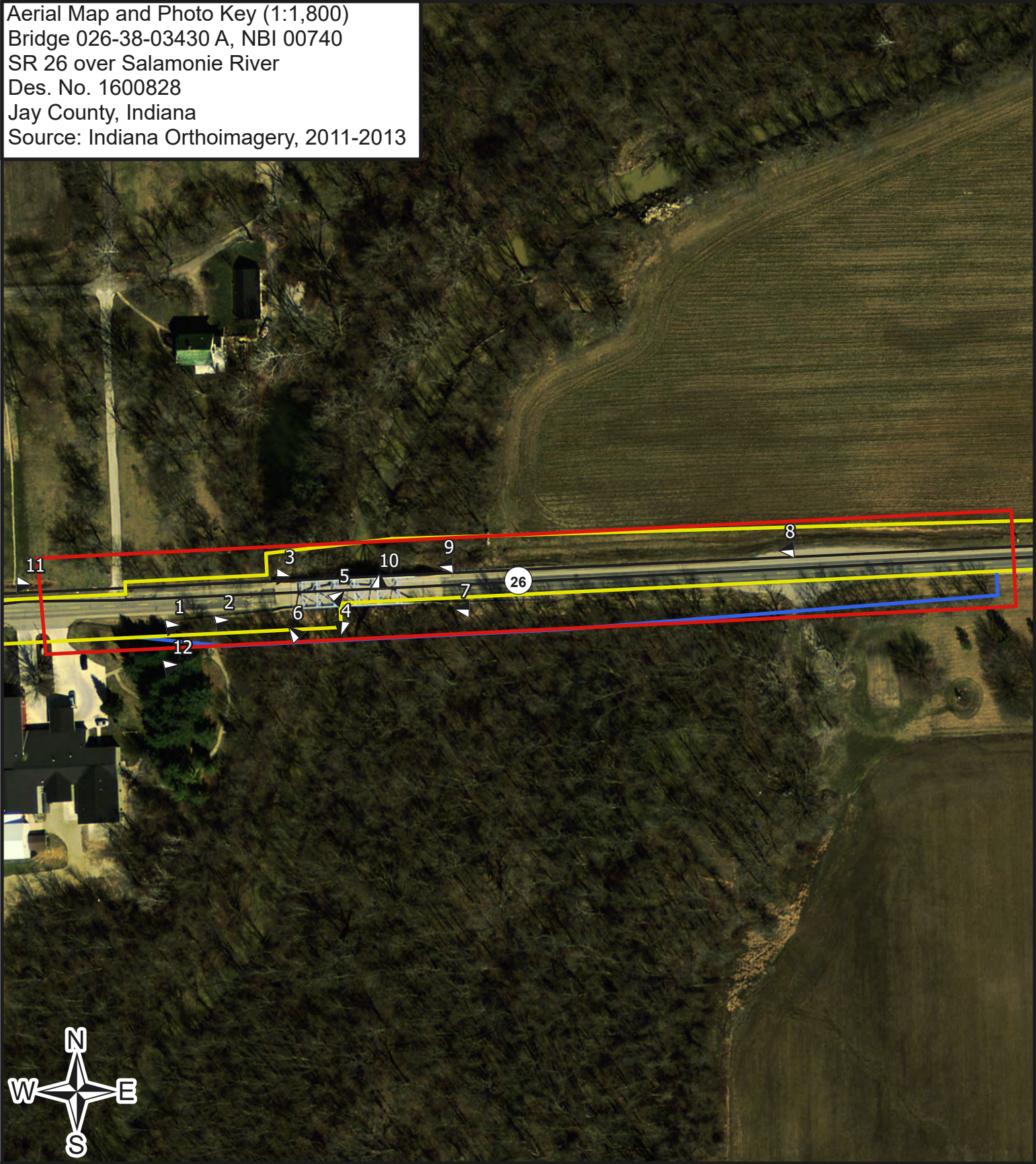
Red Flag Investigation -Site Location
SR 26 over Salamonie River, 0.78 miles East of US 27
Des. No. 1600828 , Bridge Project
Jay County, Indiana







Sources: 0.35 0.17 0 0.35 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

PORTLAND QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Aerial Map and Photo Key (1:1,800)
Bridge 026-38-03430 A, NBI 00740
SR 26 over Salamonie River
Des. No. 1600828
Jay County, Indiana
Source: Indiana Orthoimagery, 2011-2013



-  Project Location
 -  Existing Right-of-Way
 -  Proposed Right-of-Way
 -  Photo Point
- 5/3/2021





1. Facing east along SR 26 to project bridge.



2. Facing east to project bridge and riparian corridor of Salamonie River.



3. Facing east from west bank of Salamonie River, north side of project bridge.



4. Facing south to Salamonie River, south side of project bridge.



5. Facing northeast from under project bridge to Salamonie River.



6. Facing northwest from southwest quadrant of project bridge.

SR 26 over Salamonie River, City of Portland, Jay Co.

Des No. 1600828

8.19.2020 Site Photos



7. Facing west from east side of Salamonie River, south side of SR 26, to riparian vegetation adjacent to project bridge.



8. Facing west to project bridge from near project point of ending, north side of SR 26.

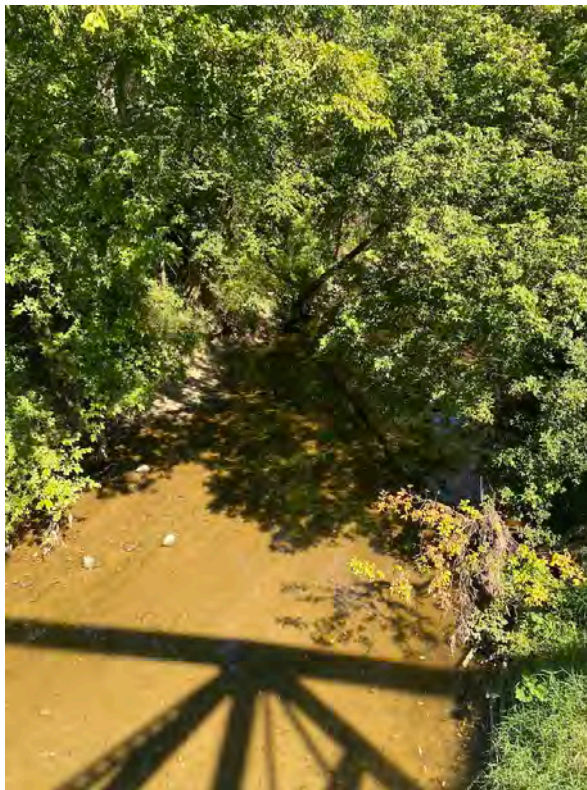
SR 26 over Salamonie River, City of Portland, Jay Co.

Des No. 1600828

8.19.2020 Site Photos



9. Facing west from east side of Salamonie River, north side of SR 26, to riparian vegetation adjacent to project bridge.



10. Facing north from project bridge to Salamonie River.



11. Facing east to project bridge from western project point of ending.



12. Exercise trail near southwest quadrant of project bridge.

PROJECT	DESIGNATION NO.
1600828	1600828
CONTRACT	BRIDGE FILE
B 39818	026-38-10192 A

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
026-38-10192 A	Continuous Composite Prestressed Concrete Bulb Tee Beam Bridge	3 Spans: 70'-0", 70'-0", 70'-0" Skew: 15° Rt.	Salamonie River	56+60.00 "SR 26-1941"

INDIANA DEPARTMENT OF TRANSPORTATION



BRIDGE PLANS FOR SPANS OVER 20 FEET

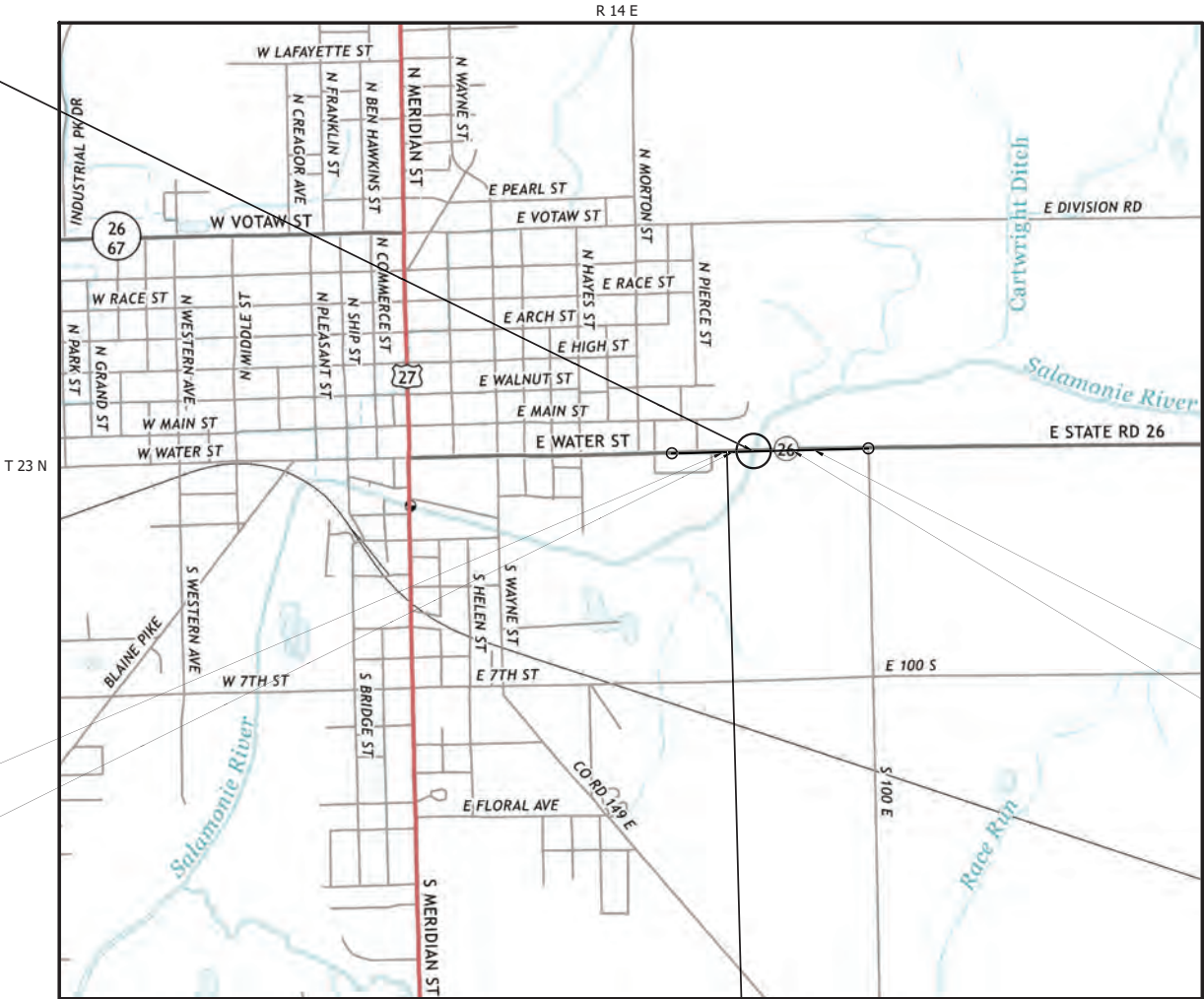
ROUTE: SR 26
PROJECT NO.

AT: RP 141+23
1600828 P.E.
1600828 CONST.
1600828 R/W

Note to Reader: Pages from this plan set were removed to reduce the overall size of this CE document and can be made available upon request.

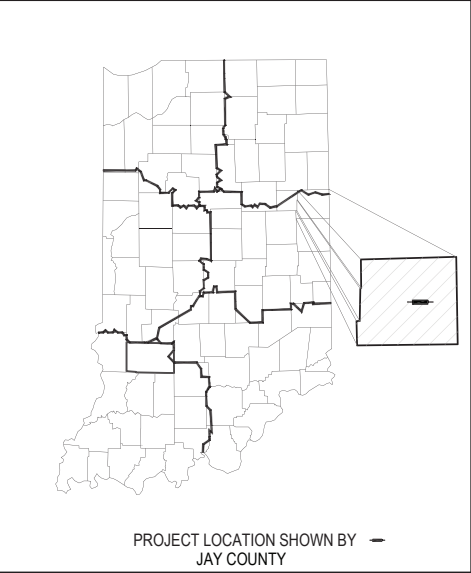
BRIDGE REPLACEMENT ON SR 26 OVER SALAMONIE RIVER
LOCATED 0.78 MILES EAST OF US 27
SECTION 21, T-23-N, R-14-E, WAYNE TOWNSHIP, JAY COUNTY, INDIANA

STR. NO. 026-38-10192 A
Project No. 1600828



LOCATION MAP

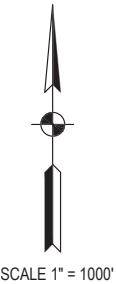
TRAFFIC DATA	
A.A.D.T. (2022)	2902 V.P.D.
A.A.D.T. (2042)	3984 V.P.D.
D.H.V. (2042)	438 V.P.H.
DIRECTIONAL DISTRIBUTION	50 %
TRUCKS	16 % D.H.V.
	16 % A.A.D.T.
DESIGN DATA	
DESIGN SPEED	40 MPH
PROJECT DESIGN CRITERIA	3R (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Major Collector
RURAL/URBAN	Urban
TERRAIN	Level
ACCESS CONTROL	None



LATITUDE: 40°25'57.17" N LONGITUDE: 84°57'48.78" W

BRIDGE LENGTH = 0.040 mi.
ROAD LENGTH = 0.156 mi.
TOTAL LENGTH = 0.196 mi.
MAX. GRADE = -1.01%

HUC: 05120102010030



END INCIDENTAL CONSTRUCTION
STA. 63+35.00 "SR 26-1941"
END PROJECT
STA. 60+90.00 "SR 26-1941"

BEGIN INCIDENTAL CONSTRUCTION
STA. 53+00.00 "SR 26-1941"
BEGIN PROJECT
STA. 54+07.90 "SR 26-1941"

REVISED STAGE 2 PLANS 10-27-2020

[INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS]



PLANS PREPARED BY:	USI Consultants, Inc.	317-544-4996
		PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:		DATE
	INDIANA DEPARTMENT OF TRANSPORTATION	

BRIDGE FILE	
026-38-10192 A	
DESIGNATION NO.	
1600828	
SHEETS	
1	of 22
PROJECT NO.	
1600828	
CONTRACT	
B 39818	

UTILITIES

AEP

8500 Smiths Mill Road
New Albany, IN 43054
Contact: Joshua Adams
PH: (614) 933-2297
Email: tl_publicprojects@aep.com

JAY COUNTY R.E.M.C.

484 S. C.R. 200 W.
Portland, IN 47371
Contact: Dwayne Muhlenkamp
PH: (260) 726-7121
Email: muhlenkampd@jayremc.com

CENTURYLINK (LOCAL)

1201 Business 30 E
Columbia City, IN 46725
Contact: Melissa Teague
PH: (765) 656-4663
Email: melissa.teague@centurylink.com

OHIO VALLEY GAS CORP.
(PORTLAND)

111 Energy Park Drive
Winchester, IN 47394
Contact: Greg Bailey
PH: (765) 584-6842
Email: gbailey@ovgc.com

CENTURYLINK (NATIONAL)

100 S. Cincinnati Ave.
Tulsa, OK 74103
Contact: Kendall Zetina
PH: (918) 547-0547
Email: kendall.zetina@centurylink.com

CITY OF PORTLAND
WASTEWATER

1315 Shadeland Drive
Portland, IN 47371
Contact: Brad Clayton
PH: (260) 726-7696
CELL: (260) 729-1914
Email: bbrelsford@thecityofportland.net

COMCAST CABLE (FORT WAYNE)

720 Taylor Street
Fort Wayne, IN 46802
Contact: Doug Fishburn
PH: (317) 516-2368
CELL: (260) 410-3504
Email: william_fishburn@cable.comcast.net

CITY OF PORTLAND

321 N. Meridian Road
Portland, IN 47371
Contact: Doug Jackson

COMMUNITY FIBER SOLUTIONS,
INC.

Contact: Joshua Rumbaugh
PH: (419) 999-2824
CELL: (419) 371-4187
Email: joshd@watchtv.net

REVISIONS

[illegible]

1-800-382-5544
CALL BEFORE YOU DIG

CAUTION !

THE LOCATIONS OF ALL EXISTING UNDERGROUND UTILITIES SHOWN ON THIS PLAN ARE BASED UPON ABOVE GROUND EVIDENCE (including, but not limited to, manholes, inlets, valves, and marks made upon the ground by others) AND ARE SPECULATIVE IN NATURE. THERE MAY ALSO BE OTHER EXISTING UNDERGROUND UTILITIES FOR WHICH THERE IS NO ABOVE GROUND EVIDENCE OR FOR WHICH NO ABOVE GROUND EVIDENCE WAS OBSERVED. THE EXACT LOCATIONS OF SAID EXISTING UNDERGROUND UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ANY AND ALL CONSTRUCTION.

INDEX

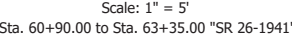
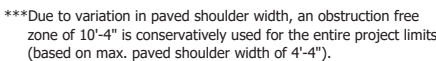
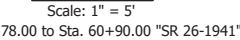
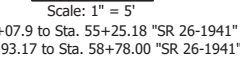
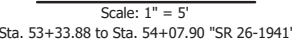
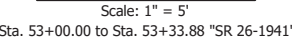
NOT FOR
CONSTRUCTION

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CHECKED: <u> BMA </u>	CHECKED: <u> JMH </u>

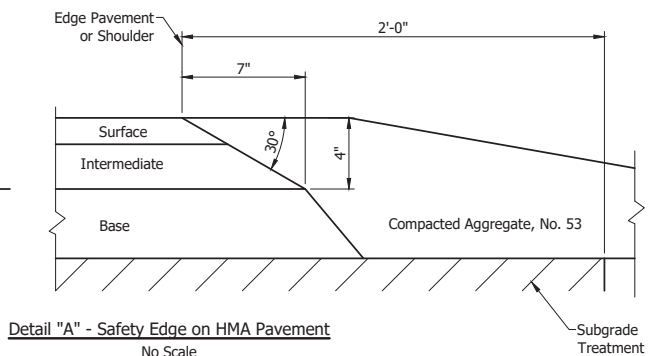
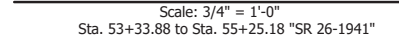
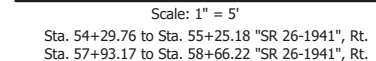
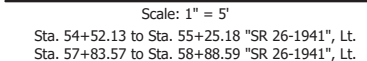
INDIANA
DEPARTMENT OF TRANSPORTATION

INDEX

HORIZONTAL SCALE	BRIDGE FILE		
NONE	026-38-10192 A		
VERTICAL SCALE	DESIGNATION		
NONE	1600828		
SURVEY BOOK		SHEETS	
	2	of	22
CONTRACT		PROJECT	
B 30818		1600828	



⑥ Varies 10'-0" from Sta. 60+90.00 to 10'-5" at Sta. 63+35.00 "SR 26-1941"



Assumed Pavement Design.
Final Pavement Design to be
provided by INDOT

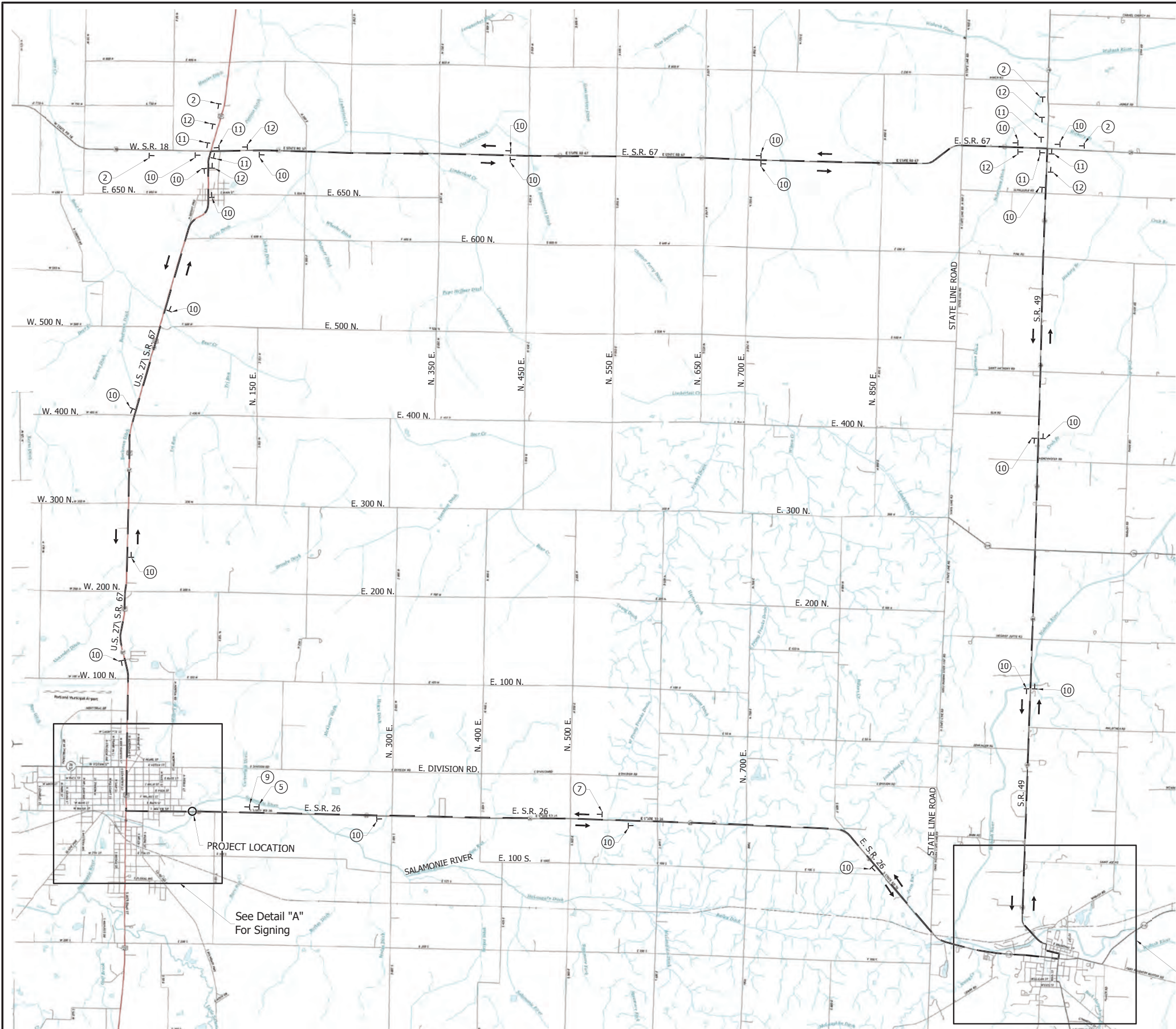
NOT FOR
CONSTRUCTION

DESIGNED: _____ JMH	DRAWN: _____ BDC
CHECKED: _____ BMA	CHECKED: _____ JMH

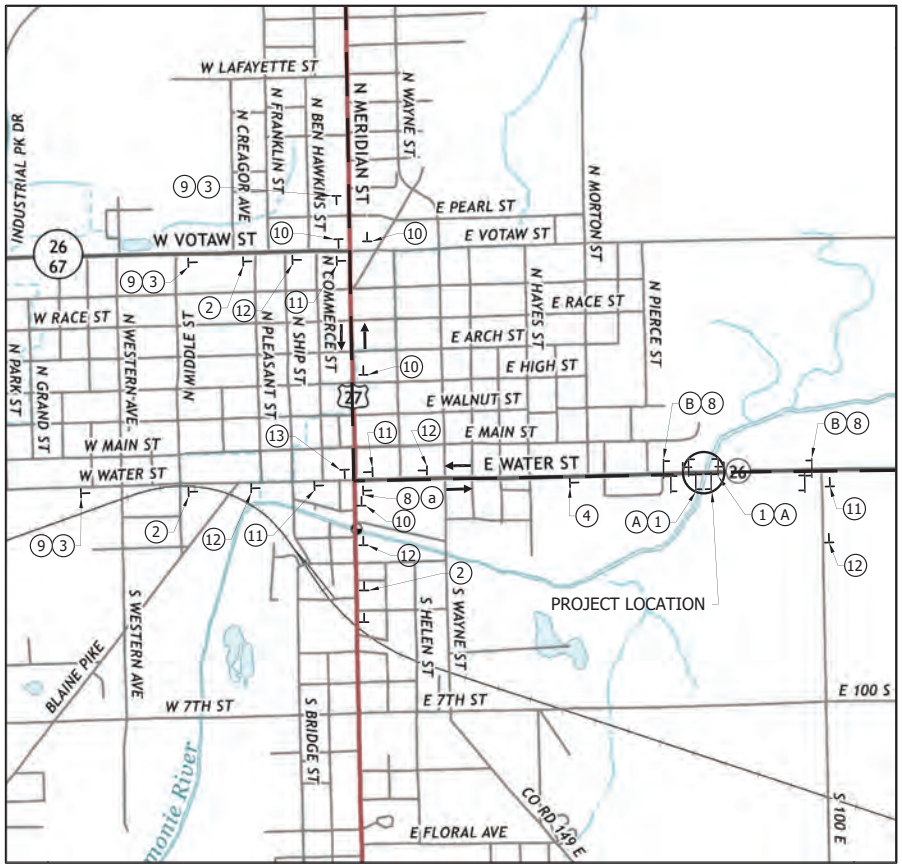
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

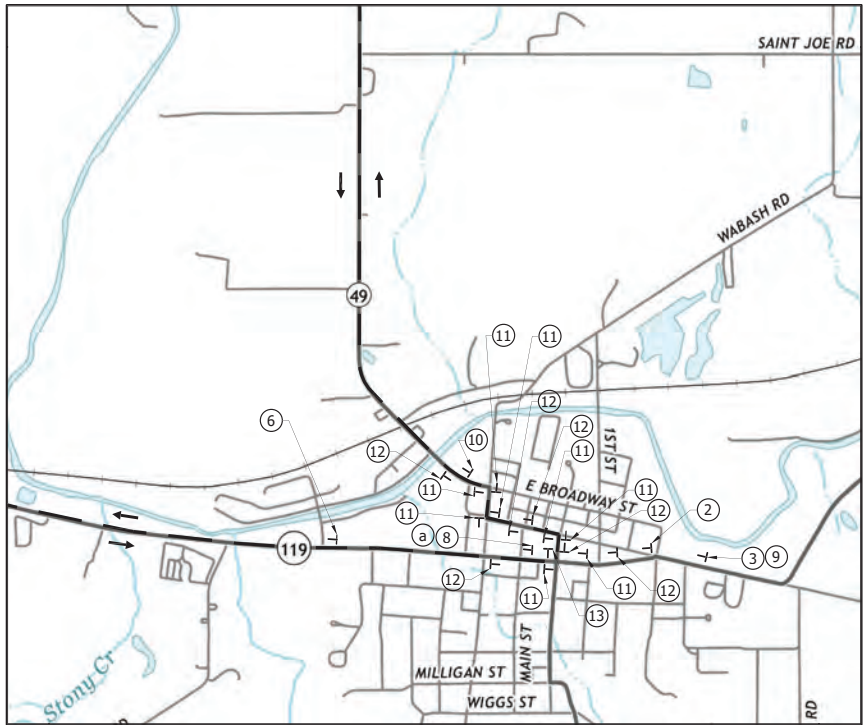
HORIZONTAL SCALE	BRIDGE FILE		
AS NOTED	026-38-10192 A		
VERTICAL SCALE	DESIGNATION		
AS NOTED	1600828		
SURVEY BOOK	SHEETS		
	3	of	22
CONTRACT	PROJECT		
B 3981R	1600R2R		



DETOUR ROUTE MAP
Scale: 1" = 3000'



DETAIL "A"
Scale: 1" = 1000'



DETAIL "B"
Scale: 1" = 1000'

See Detail "B"
For Signing








NOTES:
For signing legend and quantities see Sheet No. 5.










LEGEND	
--- Detour Route	→ Indicates Traffic Flow
⊥ Construction Sign	⊥ Type III Barricade

NOT FOR CONSTRUCTION	
DESIGNED: JMH	DRAWN: BDC
CHECKED: BMA	CHECKED: JMH

INDIANA DEPARTMENT OF TRANSPORTATION	
DETOUR ROUTE	

HORIZONTAL SCALE AS NOTED	BRIDGE FILE 026-38-10192 A
VERTICAL SCALE AS NOTED	DESIGNATION 1600828
SURVEY BOOK	SHEETS 4 of 22
CONTRACT B 39818	PROJECT 1600828

LEGEND	DESCRIPTION	SIZE	TYPE	NO.
①	 ROAD CLOSED	R11-2	48" X 30"	Road Closure Assembly 2*
②	 DETOUR AHEAD	XW20-2	48" X 48"	A 8
③	 ROAD CLOSED AHEAD	XW20-3	48" X 48"	A 4
④	 ROAD CLOSED 1500'	XW20-3 (1500')	48" X 48"	A 1
⑤	 ROAD CLOSED 4000'	XW20-3 (4000')	48" X 48"	A 1
⑥	 ROAD CLOSED X MILES	XW20-3 (10 MILES)	48" X 48"	A 1
⑦	 ROAD CLOSED X MILES	XW20-3 (5 MILES)	48" X 48"	A 1

LEGEND	DESCRIPTION		SIZE	TYPE	NO.
⑧		R11-4	60" X 30"	Road Closure Assembly	4*
Ⓐ		XM4-10 (L or R)	48" X 18"	B	34*
⑨		XW20-6	60" X 30"	A	5
⑩		XM4-8 M1-5 (SR 26) M3-2 or M3-4 M6-3	30" X 15" 24" X 24" 30" X 15" 21" X 15"	Detour Route Assembly	26
⑪		XM4-8 M1-5 (SR 26) M3-2 or M3-4 M6-1 (L or R)	30" X 15" 24" X 24" 30" X 15" 21" X 15"	Detour Route Assembly	16
⑫		XM4-8 M1-5 (SR 26) M3-2 or M3-4 M5-1 (L or R)	30" X 15" 24" X 24" 30" X 15" 21" X 15"	Detour Route Assembly	17
⑬		XM4-6 XM4-8 M1-5 (SR 26) M5-1 (L or R)	30" X 15" 30" X 15" 24" X 24" 21" X 15"	Detour Route Assembly	2
Ⓐ	TYPE III-A BARRICADE		12'		72
Ⓑ	TYPE III-B BARRICADE		12'		48

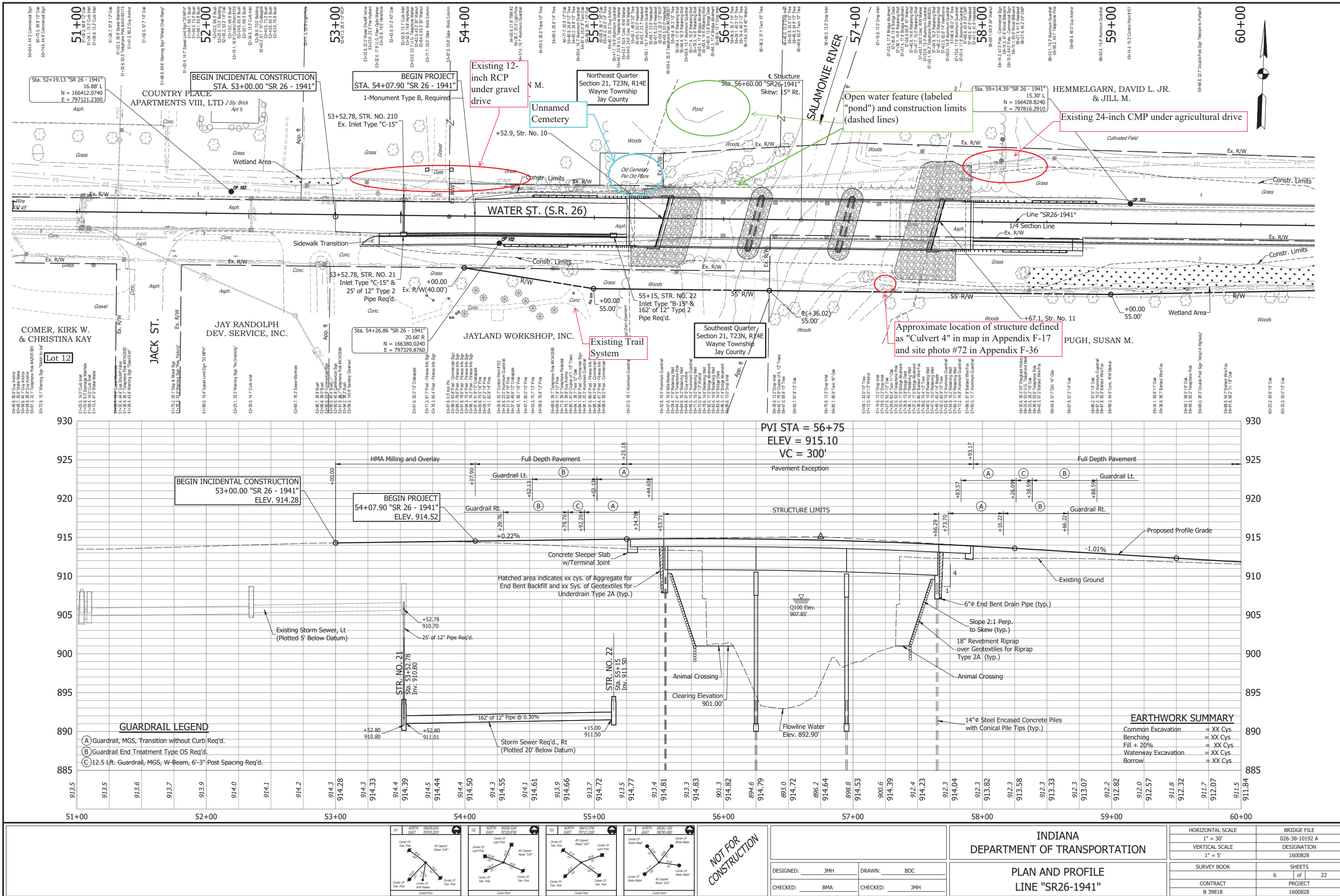
5-XW20-6 Signs to be placed at site a minimum of 10 business days prior to Road Closure.

* Cost of Sign to be included in the cost of "Road Closure Sign Assembly"

TYPE OF SIGN	QUANTITY	PAY ITEM
ROAD CLOSURE ASSEMBLY	7 Each	801-04308
DETOUR ROUTE ASSEMBLY	61 Each	801-06625
CONSTRUCTION SIGN TYPE A	26 Each	801-06640
BARRICADE, III-A	72 Lft.	801-07118
BARRICADE, III-B	48 Lft.	801-07119
MAINTAINING TRAFFIC	1 LSUM	801-06775

NOTES:
For Detour Route map and sign locations see Sheet No. 4.

	NOT FOR CONSTRUCTION			INDIANA DEPARTMENT OF TRANSPORTATION	<table><tr><td>HORIZONTAL SCALE</td><td colspan="2">BRIDGE FILE</td></tr><tr><td>AS NOTED</td><td colspan="2">026-38-10192 A</td></tr><tr><td>VERTICAL SCALE</td><td colspan="2">DESIGNATION</td></tr><tr><td>AS NOTED</td><td colspan="2">1600828</td></tr></table>		HORIZONTAL SCALE	BRIDGE FILE		AS NOTED	026-38-10192 A		VERTICAL SCALE	DESIGNATION		AS NOTED	1600828	
		HORIZONTAL SCALE	BRIDGE FILE															
		AS NOTED	026-38-10192 A															
		VERTICAL SCALE	DESIGNATION															
AS NOTED	1600828																	
DESIGNED: _____ JMH		DRAWN: _____ BDC		DETOUR ROUTE														
CHECKED: _____ BMA		CHECKED: _____ JMH																
				<table><tr><td>SURVEY BOOK</td><td colspan="2">SHEETS</td></tr><tr><td></td><td>5</td><td>of 22</td></tr><tr><td>CONTRACT</td><td colspan="2">PROJECT</td></tr><tr><td>B 39818</td><td colspan="2">1600828</td></tr></table>		SURVEY BOOK	SHEETS			5	of 22	CONTRACT	PROJECT		B 39818	1600828		
SURVEY BOOK	SHEETS																	
	5	of 22																
CONTRACT	PROJECT																	
B 39818	1600828																	

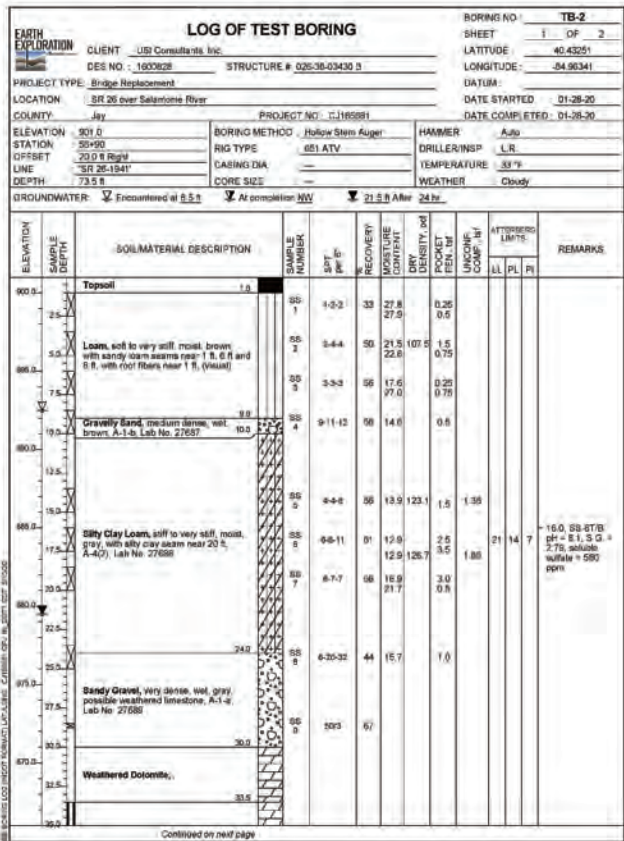
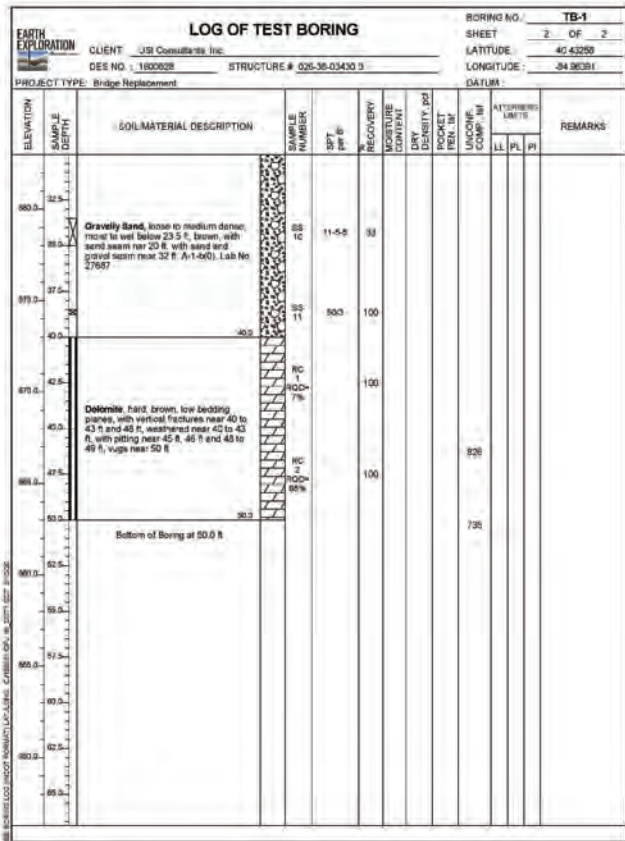
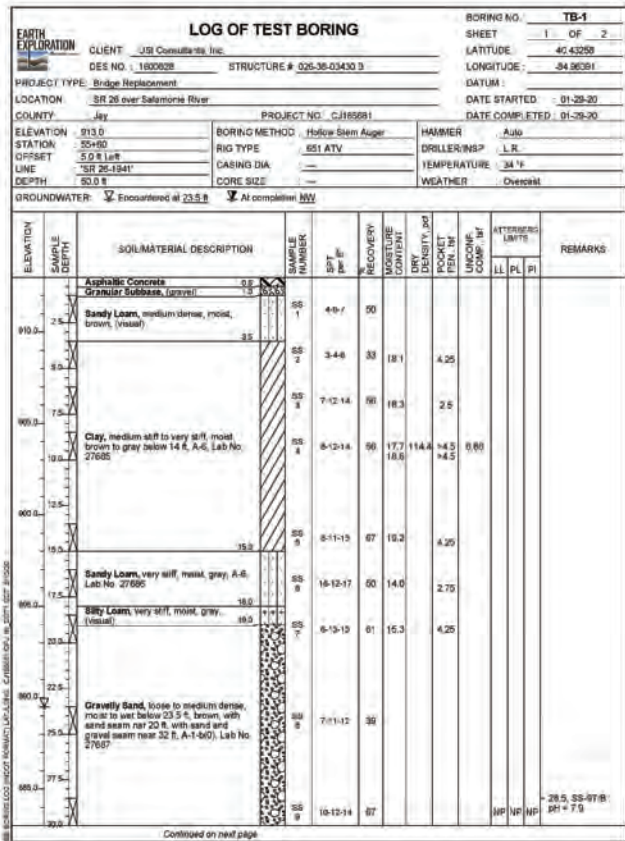


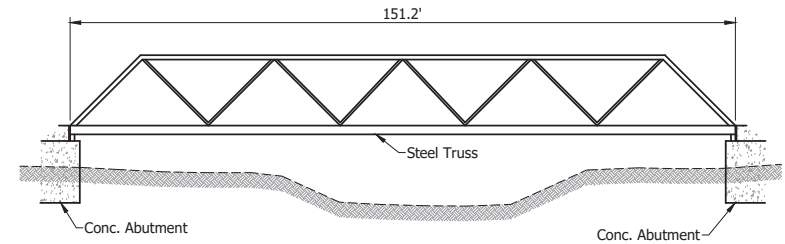
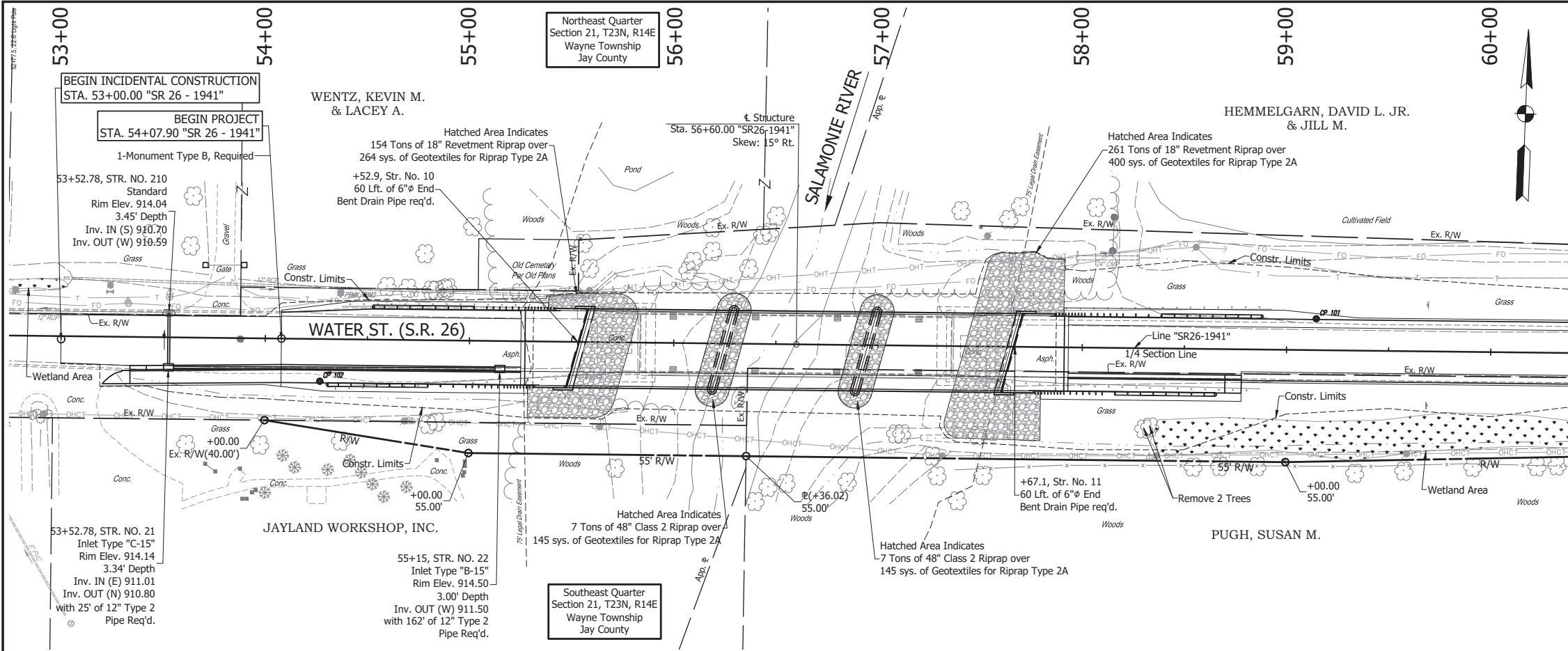




NOTE:
For Soil Boring Logs, see Sheet No. 9.

Appendix B - 17



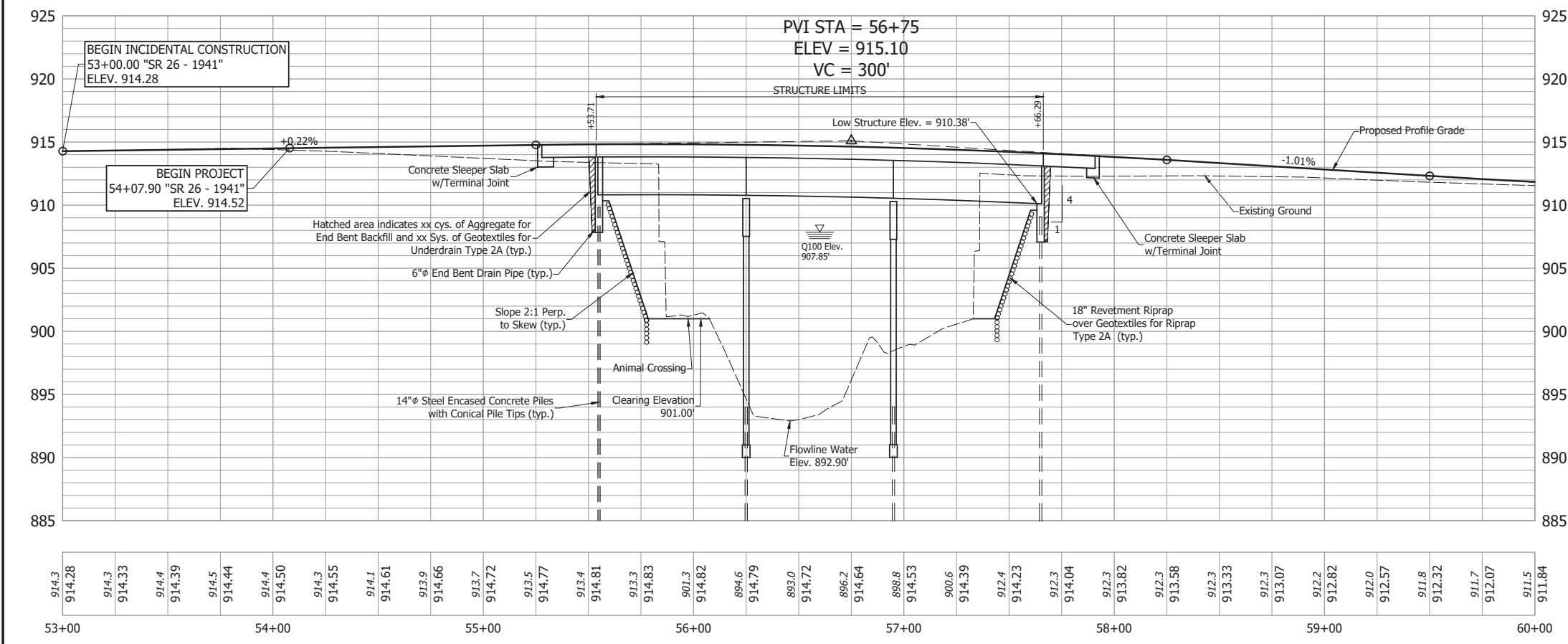


EXISTING STRUCTURE
One Span Steel Truss Bridge
(Structure to be Removed)
Not to Scale

HYDRAULIC DATA

SITE DATA:	
Drainage Area:	= 44 sq. mile
Q100 Discharge:	= 4,100 cfs
Q100 Elevation:	= 907.85 ft.
Q500 Discharge:	= 5,330 cfs
EXISTING STRUCTURE	
Existing Waterway Opening below	
Q100 Elevation (Str.):	= 1,522.16 sq. ft.
Existing Road Overflow Waterway Area:	= 0 sq. ft.
Existing Low Structure Elevation:	= 908.70 ft.
Existing Backwater:	= 0.32 ft.
Skew:	= 0 degrees
PROPOSED STRUCTURE	
Backwater:	= 0.27 ft.
Average Velocity At Q100:	= 2.40 ft./sec.
Low Structure Elevation:	= 910.38'
Gross Waterway Opening Req'd.	
Below Q100 Elevation (Str.):	= 1,735.09 sq. ft.
Road Overflow Waterway Area:	= 0 sq. ft.

Q100 SCOUR	
Q100 Contraction Scour:	= 19.34 ft.
Q100 Pier Scour Depth:	= 4.54 ft.
Q100 Total Scour Depth:	= 23.88 ft.
Q100 Low Scour Elevation:	= 869.02 ft.
Max. Velocity @ Q100:	= 3.26 ft./sec.
Q500 SCOUR	
Q500 Contraction Scour:	= 27.52 ft.
Q500 Pier Scour Depth:	= 4.82 ft.
Q500 Total Scour Depth:	= 32.34 ft.
Q500 Low Scour Elevation:	= 860.56 ft.
Max. Velocity @ Q500:	= 3.65 ft./sec.



CONTINUOUS COMPOSITE PRESTRESSED CONCRETE BULB TEE BEAM BRIDGE

THREE SPANS @ 70'-0", 70'-0", 70'-0"
SKEW: 15° RT.
28'-4" CLEAR ROADWAY WIDTH
S.R. 26 OVER SALAMONIE RIVER
JAY COUNTY, INDIANA

Note to Reader: the clear roadway width is 30 feet. This notation will be revised in subsequent plan documents

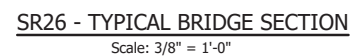
NOT FOR CONSTRUCTION

INDIANA
DEPARTMENT OF TRANSPORTATION
LAYOUT

DESIGNED: JMH	DRAWN: BDC
CHECKED: BMA	CHECKED: JMH

HORIZONTAL SCALE 1" = 30'	BRIDGE FILE 026-38-10192 A
VERTICAL SCALE 1" = 5'	DESIGNATION 1600828
SURVEY BOOK CONTRACT B 39818	SHEETS 10 of 22 PROJECT 1600828

PVI STA. 56+75.00 "SR 26-1941"
ELEV. = 915.10



THREE SPANS @ 70'-0", 70'-0", 70'-0"
SKEW: 15° RT.
28'-4" CLEAR ROADWAY WIDTH
S.R. 26 OVER SALAMONIE RIVER
JAY COUNTY, INDIANA

NOT FOR
CONSTRUCTION

STRUCTURE DATA TABLE

[illegible]

UNDERDRAIN TABLE

[illegible]

PIPE MATERIAL TABLE

STRUCTURE NUMBER		21	22	
PIPE TYPE / SHAPE		2 / CIR	2 / CIR	
SMOOTH PIPE SIZE		12"	12"	
CORRUGATED PIPE SIZE				
RCP / RCHEP (S)	CLASS			
	D ^{0.01} RATING			
NON-REINFORCED CONCRETE PIPE, CLASS 3 (S)				
CORRUGATED PE PIPE, TYPE S (S) *				
RIBBED PE PIPE (S) *				
SMOOTH WALL PE PIPE (S)* / MAXIMUM DR				
PROFILE WALL PVC PIPE (S)				
SMOOTH WALL PVC PIPE (S) *				
VITRIFIED CLAY PIPE, EXTRA STRENGTH (S)				
CORRUGATED STEEL PIPE / PIPE-ARCH	FULLY BIT. PAVED & LINED (S)	CORR. PROFILE THICKNESS		
	ZINC COATED (C)	CORR. PROFILE THICKNESS		
	ZINC COATED W/ BPI (C)	CORR. PROFILE THICKNESS		
	ALUM. COATED TYPE 2 (C)	CORR. PROFILE THICKNESS		
	ALUM. COATED TYPE 2 W/ BPI (C)	CORR. PROFILE THICKNESS		
	POLYMER PRECOATED GALVANIZED (C)	CORR. PROFILE THICKNESS		
	POLYMER PRECOATED GALVANIZED W/ BPI (C)	CORR. PROFILE THICKNESS		
	FIBER BONDED BITUMINOUS COATED (C)	CORR. PROFILE THICKNESS		
	FIBER BONDED BITUMINOUS COATED W/ BPI (C)	CORR. PROFILE THICKNESS		
	CORRUGATED ALUM. ALLOY PIPE (C)	CORR. PROFILE THICKNESS		
	CORRUGATED ALUM. ALLOY PIPE W/ BPI (C)	CORR. PROFILE THICKNESS		
	STR. PLATE ALUMINUM ALLOY PIPE (C)	CORR. PROFILE THICKNESS		
	STR. PLATE ALUMINUM ALLOY PIPE W/ CFP (C)	CORR. PROFILE THICKNESS		
	STR. PLATE STEEL PIPE (C)	CORR. PROFILE THICKNESS **		
	STR. PLATE STEEL PIPE W/ CFP (C)	CORR. PROFILE THICKNESS **		

LEGEND

RCP-	REINFORCED CONCRETE PIPE
RCHEP-	REINFORCED CONCRETE HORIZONTAL ELLIPTICAL PIPE
PE-	POLYETHYLENE
DR-	DIMENSION RATIO
PVC-	POLYVINYL CHLORIDE
BIT-	BITUMINOUS
CORR-	CORRUGATION
BPI-	BITUMINOUS PAVED INVERT
ALUM-	ALUMINUM
STR-	STRUCTURAL
CFP-	CONCRETE FIELD PAVING
CIR-	CIRCULAR PIPE
DEF-	DEFORMED PIPE
(S)-	SMOOTH PIPE MATERIAL
(C)-	CORRUGATED PIPE MATERIAL
OK-	ACCEPTABLE FOR USE
(LS)-	LOCK SEAM PIPE REQUIRED
*-	REFER TO STANDARD DRAWING 715-PHCL-18 OR 19 FOR NOMINAL DIAMETER APPROPRIATE FOR PAY ITEM DIAMETER
**-	TABULATED THICKNESS REFERS TO TOP & SIDE PLATES. BOTTOM PLATES SHALL BE OF NEXT GREATER AVAILABLE THICKNESS.

NOT FOR
CONSTRUCTION

DESIGNED: _____ JMH	DRAWN: _____ BDC
CHECKED: _____ BMA	CHECKED: _____ JMH

INDIANA
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS TABLES

HORIZONTAL SCALE	BRIDGE FILE		
NONE	026-38-10192 A		
VERTICAL SCALE	DESIGNATION		
NONE	1600828		
SURVEY BOOK	SHEETS		
	12	of	22
CONTRACT	PROJECT		
B 39818	1600828		

S:\2017\03\2017-102 SR 26\Final Bridge Plans\2017-102 Bridge Summary.dwg, 13 Bridge Summary, Tue Oct 27 10:48:51, 2020

SUMMARY OF BRIDGE QUANTITIES																																
ITEM	CONCRETE			CONCRETE RAILING CLASS C		REINF. STEEL	EPOXY COATED REINF. STEEL	PILES										SURFACE SEAL **	RAILING PS-1	RAILING PF-1	CONCRETE BRIDGE RAILING TRANSITION, TPS-1	CONCRETE BRIDGE RAILING TRANSITION, TPF-1	SUBBASE FOR PCCP	R.C. BRIDGE APPROACH (12")	CONC. STR. MEMBERS	CAST IRON GRATES, BASINS & FITTINGS	CAST IRON DRAIN PIPE, 6"φ	TERMINAL JOINT, TYPE HMA				
	CLASS C	CLASS A	CLASS B IN FTG.					14"φ CONC. STEEL SHELL ENCASED		14"φ CONC. STEEL SHELL ENCASED EPOXY COAT.		STEEL H HP12 x 53		STEEL H EPOXY COATED		STEEL H REINF. CONC. ENCASED									PILE TIP STEEL H					CORED HOLES IN ROCK		PRESTRESSED CONCRETE 36 X 49 BULB-TEE
	superstr	substr		cys	lft.	lbs.	lbs.	no.	lft.	no.	lft.	no.	lft.	no.	lft.	no.	lft.	each	no.	lft.	sft.	lft.	lft.	each		each	cys.	sys.	lft.	each	lbs.	
SUPERSTRUCTURE	xx			x			x													x							x		x			
BENT NO. 1		x					x	x	x											x												
PIER NO. 2		x					x	x	x	x																						
PIER NO. 3		x					x	x	x	x																						
BENT NO. 4		x					x	x	x											x												
WEST APPROACH SLAB							x													x			x		x						x	
EAST APPROACH SLAB							x													x			x		x						x	
TOTALS	x	x		x			x		x		x									x	x	x	x	x	x	x	x		x	x	x	

** ESTIMATED QUANTITY

PAVEMENT QUANTITIES AND APPROACH TABLE																																								
LOCATION	DESCRIPTION (APPROACH TYPE OR CLASS)	WIDTH	LENGTH	RADI	DISTANCE BEYOND R/W LINE	SURFACE BEYOND R/W LINE			GRADE		EXCAVATION		CLEAR ZONE AT DRIVE	QC/QA-HMA, 3, 64						QC/QA-HMA, 2, 64						HMA SURFACE 9.5 mm, SHLDR.		HMA BASE 25.0 mm, SHLDR.		BITUMINOUS MATERIAL FOR		COMPACTED AGGREGATE FOR SHLDR. NO. 53		COMPACTED AGGREGATE FOR SURFACE NO. 73		SUBGRADE TREATMENT TYPE IC	SUBGRADE TREATMENT TYPE xxx	JOINT ADHESIVE, SURFACE	JOINT ADHESIVE, INTERMEDIATE	JOINT SEALANT
						COMPACTED AGGREGATE BASE	HMA	CONCRETE						SURFACE 9.5 mm		INTERMD. 19.0 mm		BASE 25.0 mm		SURFACE 9.5 mm		INTERMD. 19.0 mm		BASE 25.0 mm																
														1	2	CUT	FILL	1	2	1	2	1	2	1	2															
						tons	tons	sft.	%	%	cys	cys	ft.	lbs./syd.	tons	lbs./syd.	tons	lbs./syd.	tons	lbs./syd.	tons	lbs./syd.	tons	lbs./syd.	tons	lbs./syd.	tons	syd	tons	tons	tons	tons	tons	tons	tons	tons	tons	tons	tons	tons
Sta. 53+00.00 to Sta. 55+25.18 "SR 26-1941"	Mainline													x	x							x	x	x	x					x				x		x		x		
Sta. 57+93.17 to Sta. 63+35.00 "SR 26-1941"														x	x							x	x	x	x					x				x		x		x		
TOTALS															x								x		x	x			x					x		x		x		

APPROACH STRUCTURES																							
STRUCTURE NUMBER	LOCATION				SIZE	DESCRIPTION		LENGTH	SKEW	FLOW LINE			BACKFILL TYPE	STRUCTURE BACKFILL	REVTMENT RIPRAP	CONCRETE CLASS "A"	PIPE END SECTION	AGGREGATE FOR END BENT BACKFILL	GEOTEXTILES	GRATED BOX END SECTION		CONNECT TO STR.	REMARKS
	STATION	LEFT	RIGHT	CROSS		TYPE	KIND			COVER	UP STREAM	DOWN STREAM											
	inches																						
	lft.	ft.	elev.	elev.	cys.	tons	cys.	ea.	cys.	sys.	type	slope											
10	xx+xx "A"		X		3	End Bent Drain Pipe	x									x	x						
11	xx+xx "A"		X		3	End Bent Drain Pipe	x									x	x						

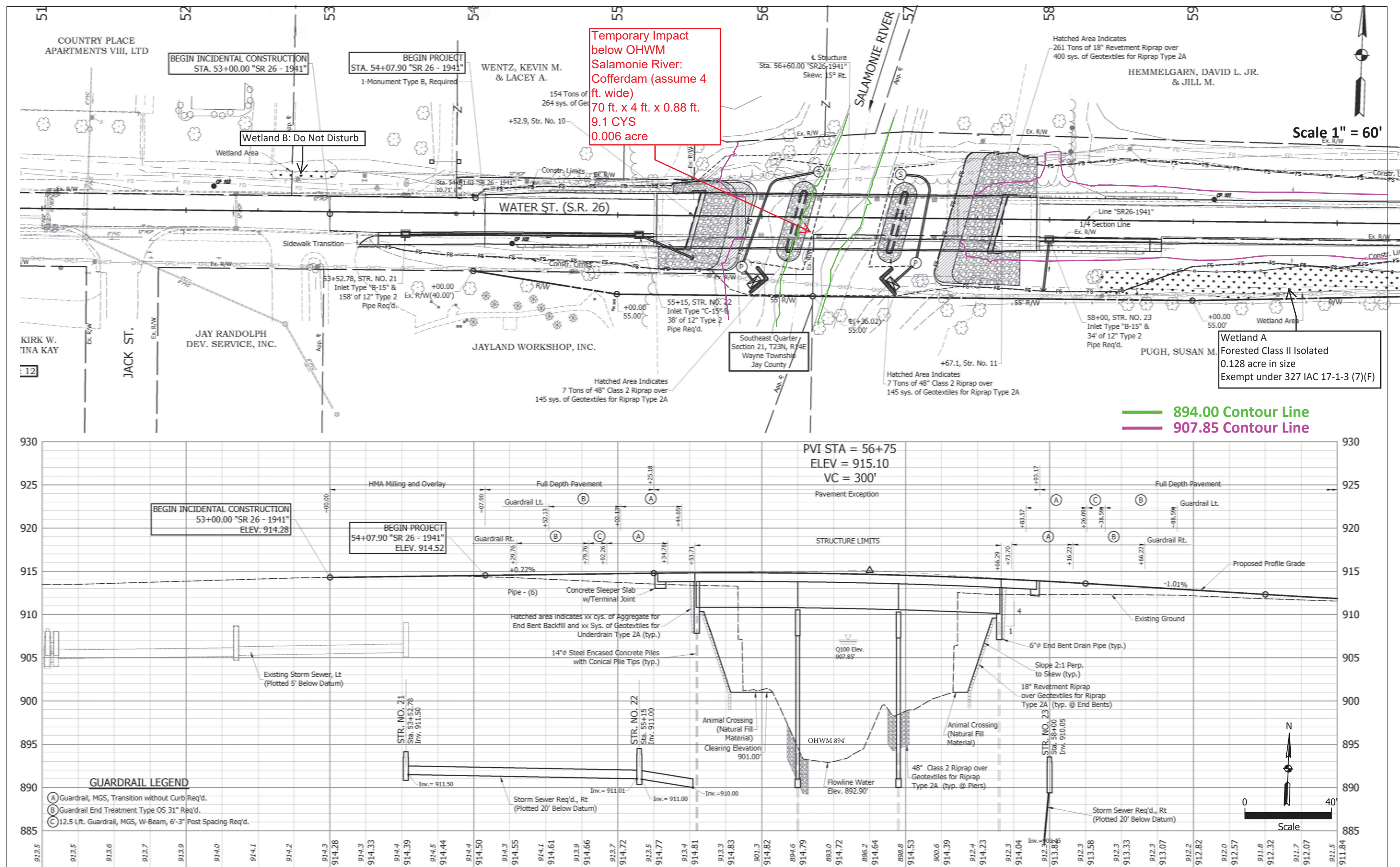
GUARDRAIL SUMMARY TABLE												
LOCATION												
FROM STATION	TO STATION	LEFT	MEDIAN LEFT	MEDIAN RIGHT	RIGHT	MGS GUARDRAIL TRANSITION WITH OUT CURB	GUARDRAIL MGS W-BEAM, 3'-6" SPACING	GUARDRAIL TRANSITION, TGB	RAILING TS-1 NESTED	W-BEAM GUARDRAIL SYSTEM TYPE 1	GUARDRAIL END TREATMENT, TYPE OS	TYPE 5 ANCHOR
						EACH	LFT	EACH	LFT	EACH	EACH	EACH
54+52.13	55+44.65	X				1					1	
54+29.76	55+34.78				X	1	12.5				1	
57+83.57	58+88.59	X				1	12.5				1	
57+73.70	58+66.22				X	1					1	
TOTALS						4	25.0				4	

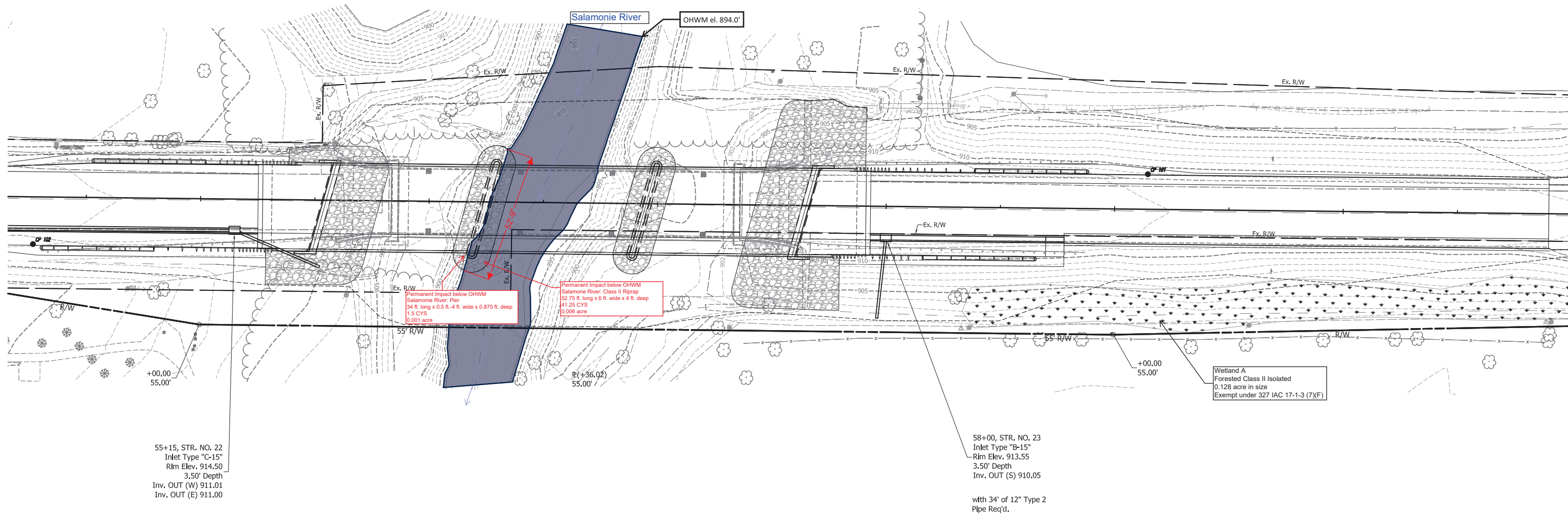
TEMPORARY EROSION AND SEDIMENT CONTROL TABLE			
LOCATION		Temporary Silt Fence	Temporary Ditch Check, Straw Bales
STATION TO STATION PLACED AT CONSTRUCTION LIMITS	LT./RT.		
xx+xx TO xx+xx "A"	LT.	xxx Lft.	
xx+xx TO xx+xx "A"	RT.	xxx Lft.	
xx+xx TO xx+xx "A"	LT.	xxx Lft.	
xx+xx TO xx+xx "A"	RT.	xxx Lft.	
TOTALS		xxx Lft.	

BENCHMARK	
QTY.	LOCATION
1	SEE SPECIAL PROVISIONS

MONUMENT TABLE		
STATION	OFFSET	MONUMENT
54+07.90 "SR 26-1941"	0.00 RT.	Type B
60+90.00 "SR 26-1941"	0.00 RT.	Type B

	NOT FOR CONSTRUCTION			INDIANA DEPARTMENT OF TRANSPORTATION		ROAD AND BRIDGE SUMMARY		HORIZONTAL SCALE NONE		BRIDGE FILE 026-38-10192 A	
		DESIGNED: JMH		DRAWN: BDC				VERTICAL SCALE NONE		DESIGNATION 1600828	
		CHECKED: BMA		CHECKED: JMH				SURVEY BOOK		SHEETS 13 of 22	
								CONTRACT B 39818		PROJECT 1600828	





	NOT FOR CONSTRUCTION			INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1"=20'		BRIDGE FILE 026-38-10192 A	
						VERTICAL SCALE		DESIGNATION 1600828	
						SURVEY BOOK		SHEETS	
						CONTRACT B 39818		PROJECT 1600828	
		DESIGNED: JMH		DRAWN: BDC				10 of 22	
CHECKED: BMA		CHECKED: JMH		Permits Drawing LINE "SR26-1941"					

Appendix C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness,
Commissioner

Note to Reader: The site photos and maps in Appendix B were sent with this sample early coordination letter.

August 25, 2020

Re: INDOT Designation No: 1600828

Bridge No: 026-38-10192

Location: East side of the City of Portland, Wayne Township 23 N, Range 14 E, Section 21

Description: S.R. 26 Bridge over Salamonie River; Jay County

Environmental Reviewer,

The Indiana Department of Transportation (INDOT) and Federal Highway Administration intend to proceed with bridge improvement project at the SR 26 bridge over Salamonie River (Bridge No. 026-38-10192; NBI No. 007040). This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number (Des. 1600828) and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

This project is located at S.R. 26 over Salamonie River, located 0.78 mile east of SR 27, on the east side of the City of Portland, Jay County. This section of S.R. 26 is a Rural Major Collector. The existing roadway approach cross section consists of two lanes approximately 11 feet in width. Bridge No. 026-38-10192 is a one span steel Parker through truss bridge constructed in 1941. The bridge is listed as eligible for the National Register of Historic Places and is a Non-Select bridge. The structure has an out-to-out coping of 29 feet with a clear roadway width of 28 feet and spans 150 feet over the Salamonie River. The vertical clearance is 14.64 feet. The structure is surrounded by the riparian corridor of the river, with a cemetery within the project area and a school adjacent to the project area to the west of the bridge and agricultural fields adjacent to the project area to the east. Several utilities are located within the project area.

The need for this project arises from the condition of the bridge. The deck wearing surface, superstructure, and substructure of the bridge are rated 5 out of 9 (fair condition). The bridge deck shows longitudinal and transverse cracking, and the wearing surface has numerous cracks over each interior floor beam. The underside of the concrete deck is supported with metal stay in place forms that exhibit several areas of corrosion at the corners, especially at the northeast end of the deck and along the edges of the floor beam upper flanges near the copings. The stringers of the superstructure show minor to moderate section loss to flanges and webs of fascia stringers in the end panels. All floor beams have some pitting, rust, and/or deterioration. The vertical and diagonal bridge members, lower chords, upper chords, end posts, gusset plates (vertical), and connection plates show varying degrees of corrosion, pitting and section loss. The abutments of the substructure exhibit horizontal and vertical cracks, delamination, and spalls. The non-standard steel bridge rail is in fair condition with corrosion at the connections and section loss holes at the southeast and northwest corners. The curbs have numerous spalls with exposed reinforcement.

The need of this project is to restore the crossing of SR 26 over the Salamonie River to a satisfactory condition and improve the safe carrying capacity of the bridge from a current 28 tons to 36 tons (HS Operating Rating). The project will require the acquisition of approximately 1.75 acres of permanent right-of-way (no temporary right-of-way is proposed). The anticipated maintenance of traffic is closure and a detour utilizing SR 49, SR 67, and US 27/SR 67. Permits from IDEM, U.S. Army Corps of Engineers, and IDNR are anticipated due to impacts to the Salamonie River. Coordination will occur with INDOT Ecology and Waterway Permitting specialists to determine permit requirements. A Wetland Delineation and Waters of the U.S. Determination Report will be completed. The project should qualify for the USFWS Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat and project information will be submitted through the USFWS Information for Planning and Consultation (IPaC) separately. Due to the structure's listing as NRHP eligible and Non-Select, the project will comply with the Historic Bridge Programmatic Agreement among the Federal Highway Administration, INDOT, the Indiana State Historic Preservation Officer (SHPO), and the Advisory Council on Historic Preservation.

Please respond with comments, questions, and concerns within thirty (30) calendar days from the date of this letter; if no response is received, it will be assumed that your agency feels that there are no adverse effects incurred as a result of this proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact Erin Mulryan at Green 3, LLC at erin@green3studio.com or 317-634-4110, or INDOT Project Manager Jeremy Greene, jegreene@indot.in.gov, 317-467-3472. Thank you in advance for your input on this project.

Sincerely,



Erin Mulryan, MPA
Green 3 LLC

Enclosures:

Early Coordination Mailing List

Project Graphics

Project Photographs



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-XXXX
FAX: (317) 233-XXXX

Eric Holcomb, Governor
Joe McGuinness, Commissioner

August 25, 2020

TO: MS4 COORDINATOR
City of Portland
118 S. Meridian St, Suite E
Portland, IN 47371

FROM: Erin Mulryan, MPA
Green 3, LLC
1104 Prospect St
Indianapolis, IN 46203

RE: Early Notification
INDOT DES Number: 1600828
Location: East side of the City of Portland, Wayne Township 23 N, Range 14 E, Section 21
Description: Bridge Project

EXAMPLE EARLY COORDINATION LETTER

Note: The ROW stated in the early coordination letters and Section 106 documentation were preliminary estimates. The ROW acquisition described in this CE document are the most current amounts.

The Indiana Department of Transportation (INDOT) (or Project Sponsor) and Federal Highway Administration (FHWA) intend to proceed with the above project. You are being notified because this project lies within an Urbanized Area Boundary (UAB). In accordance with 327 IAC 15-13 (Rule 13 - Municipal Separate Storm Sewer Systems), INDOT has developed a Storm Water Quality Management Plan (SWQMP).

As part of its implementation, projects falling within the UAB will be required to consider appropriate post construction storm water quality best management practices (BMPs). These BMPs should take into consideration the available space, pollutants of concern and receiving waters.

This letter is for notification purposes only, and no action is required by you; however, if you would like to provide your input on water quality concerns, please provide this information **within thirty (30) calendar days** from the date of this letter to the undersigned. Should we not receive your response within the specified timeframe, it will be assumed that your agency does not have additional concerns about water quality issues resulting from the proposed project. Should you find that an extension to the response time is necessary, a reasonable amount of time may be granted upon request. If you have any questions regarding this matter, please feel free to contact (Name), (Title), at (Phone Number). Thank you in advance for your attention to this matter.

Sincerely,
Erin Mulryan, MPA
President
Green 3, LLC

SR 26 Over Salamonie River, Des. No. 1600828, Jay County
Early Coordination Notice sent to the Following Agencies:

Federal Highway Administration
Greenfield District
robert.dirks@dot.gov

Indiana Geological Survey
On-Line Submission
<https://igs.indiana.edu/eAssessment/>

IDEM Groundwater Section
ATurnbow@idem.IN.gov

Christie Stanifer
Environmental Coordinator
IDNR, Div. of Fish & Wildlife
environmentalreview@dnr.in.gov

IDEM
On-Line Submission
<http://www.in.gov/idem/5284.htm>

INDOT
Manager, Public Hearings
rlark@indot.in.gov

US Dept. of HUD
Paul.J.Lehmann@hud.gov

NPS-Midwest Regional Office
Hector Santiago
Hector_santiago@nps.gov

INDOT Greenfield District
Taylor Darrah
TDarrah@indot.IN.gov

Field Supervisor
U.S. Fish and Wildlife Service
Bloomington Office
Robin McWilliams
robin_mcwilliams@fws.gov
Sent to Northern Office 1/11/2021:
elizabeth_mccloskey@fws.gov

State Conservationist NRCS
Indianapolis Office
Rick Neilson
rick.neilson@in.usda.gov

INDOT Aviation
JCourtade@indot.in.gov

USACE
Louisville District
Gregory.A.McKay@usace.army.mil
Sent 1/11/2021 to:
regulatoryapplicationsrl@usace.army.mil

US Coast Guard, 8th District
Eric.Washburn@uscg.mil

Mayor of the City of Portland
John Boggs
mayorboggs@thecityofportland.net

Jay County Surveyor
Bradley A. Daniels
surveyor@co.jay.in.us

Jay Co. Highway Department
Donnie Corn
highway@co.jay.in.us

Jay Co. Commissioner Chad Aker
Middle District (serving Knox, Green, Wayne, Noble Townships)
ch_aker@yahoo.com

Jay County Schools- Transportation
mstephen@jayschools.k12.in.us

East Elementary (located at west end of project area)
eastelem@jayschools.k12.in.us
jgregg@jayschools.k12.in.us

Jay Co. Floodplain Administrator
jpbp.jhemmel@gmail.com

MS4 Coordinator, City of Portland
Robert Brelsford
bbrelsford@thecityofportland.net

*Project is located within a WHPA
Portland Municipal Water Plant
Doug Jackson, djackson@thecityofportland.net

Superintendent of Streets and Parks
City of Portland Parks
215 S. Wayne St
Portland, IN 47371
streetsuperintendent@thecityofportland.net

Sent 1/22/2021:
Jay Co. Visitors & Tourism Bureau
director@visitjaycounty.com

Tri-State Gas Engine & Tractor Show
tristategasengine@gmail.com
(Also submitted inquiry through show website,
<https://tristategasenginetractor.com/contact.php>)

Jay County Fairgrounds
jaycountyfair@gmail.com

Subject: RE: SR 26 Over Salamonie River, Des. No. 1600828, Jay County
Date: Tuesday, August 25, 2020 at 8:22:29 PM Eastern Daylight Time
From: Turnbow, Alisha
To: Erin Mulryan
Attachments: image002.jpg, image003.png, image004.png, image005.png, image006.png, image009.png, image001.png

Hi Erin,

The project Des No 1600828 is located in Portland Municipal Water Plant's Wellhead Protection Area. The contact for Portland Municipal Water Plant is Doug Jackson and they can be reached at djackson@thecityofportland.net and 260-726-4525. Let me know what questions you have.

Sincerely,

COVID-19 Resources:

- **Indiana State Dept. of Health (ISDH) COVID-19 Call Center:** Call 877-826-0011 (available 8:00 am-5:00 pm daily).
- **Anthem NurseLine:** Call 800-337-4770 or visit the [Anthem NurseLine](#) online for a FREE symptom screening. Available to anyone with an Anthem health plan (this includes State of IN employees)
- **Anthem Employee Assistance Program (EAP):** Available to full-time state employees and their household members regardless of health plan participation. Call 800-223-7723 or visit anthemeap.com (enter State of Indiana) for crisis counseling, help finding child/elder care, legal/financial consultation and much more.



Alisha Turnbow
Environmental Manager
Office of Water Quality
Drinking Water Branch, Groundwater Section
(317) 233-9158 • aturnbow@idem.IN.gov

Indiana Department of Environmental Management



IDEM values your feedback.

Please take two minutes and complete this brief survey.



From: Erin Mulryan <erin@green3studio.com>
Sent: Wednesday, August 12, 2020 10:26 AM
To: Turnbow, Alisha <ATurnbow@idem.IN.gov>
Subject: SR 26 Over Salamonie River, Des. No. 1600828, Jay County

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Subject: RE: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination
Date: Wednesday, August 26, 2020 at 7:49:24 AM Eastern Daylight Time
From: Courtade, Julian
To: Erin Mulryan
Attachments: image002.png, image003.png, image004.png, image005.png, image006.png, image007.png, image008.png

Erin –

After reviewing the Early Coordination Letter, I have determined that if any object, obstruction, or equipment will exceed 95 ft. in height, further coordination will be required with our office. This is due to the close proximity of Portland Municipal Airport and the need for any obstructions within 5 miles to meet a 100:1 glideslope to the nearest runway. Please let me know if you have any questions!

Best,

Julian L. Courtade

Chief Airport Inspector

100 North Senate Ave, N955

Indianapolis, IN 46204

Cell: (317) 954-7385

Email: jcourtade@indot.in.gov



From: Erin Mulryan <erin@green3studio.com>
Sent: Tuesday, August 25, 2020 6:15 PM
To: Courtade, Julian <JCourtade@indot.IN.gov>
Subject: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination

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Dear Environmental Reviewer,

Subject: Re: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination
Date: Wednesday, August 26, 2020 at 8:55:15 AM Eastern Daylight Time
From: Melissa Stephen
To: Erin Mulryan
Attachments: image001.png

Good morning! I do not see an attachment.

On Tue, Aug 25, 2020 at 6:59 PM Erin Mulryan <erin@green3studio.com> wrote:

Ms. Stephen,

Hello, attached is early coordination project information for the abovementioned project on the east side of Portland, Jay County, IN for review and comment. Please feel free to contact me via phone or email. Your response is kindly requested within 30 days.

Thank You,

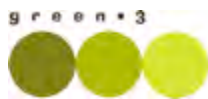
Erin Mulryan, MPA

Green 3 LLC

317-634-4110

(Due to the coronavirus, I am working from home and can be reached on my cell, 317-525-1192 if needed)

green3studio.com



--

Melissa Stephen
Transportation Director
Jay School Corporation
260-726-5272



Subject: RE: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination
Date: Tuesday, September 1, 2020 at 12:52:16 PM Eastern Daylight Time
From: Darrah, Taylor N
To: Erin Mulryan
CC: bramiller1
Attachments: image008.png, image009.png, image010.png, image011.png, image012.png, image013.png, image014.png, image015.png, image016.png, image017.png, image018.png, image019.png, image020.png

Erin,

Because the facility is a private non-profit organization and their major purpose is not for park, recreation, or refuge, the property is not Section 4(f) land.

Thank you,

Taylor Darrah

Environmental Section Manager

Indiana Department of Transportation

32 South Broadway

Greenfield, IN 46140

Office: (317) 467-3915

Cell: (317) 526-6080 – **Please temporarily direct all calls to my cell phone**

Email: TDarrah@indot.in.gov

 Go Green, There is no Planet B

From: Erin Mulryan <erin@green3studio.com>
Sent: Thursday, August 27, 2020 2:37 PM
To: Darrah, Taylor N <TDarrah@indot.IN.gov>
Cc: Miller, Brandon <BraMiller1@indot.IN.gov>
Subject: Re: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination

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The trail is open to the public as far as I can tell- no fences or other restricted point of entry. We parked there and walked around it. The [establishment](#) provides assistance to people with special needs. Based on the plans I have, the trails won't be impacted but R/W acquisition from the property is likely.

Thank You,
Erin Mulryan, MPA
Green 3 LLC
317-634-4110

(Due to the coronavirus, I am working from home and can be reached on my cell, 317-525-1192 if needed)
green3studio.com

From: "Darrah, Taylor N" <TDarrah@indot.IN.gov>
Date: Thursday, August 27, 2020 at 7:40 AM
To: Erin Mulryan <erin@green3studio.com>
Cc: bramiller1 <bramiller1@indot.in.gov>
Subject: RE: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination

Erin,

What is the establishments major purpose? Is it for park, recreation, or refuge activities and is it open to the public?

Thank you,

Taylor Darrah
Environmental Section Manager
Indiana Department of Transportation
32 South Broadway
Greenfield, IN 46140
Office: (317) 467-3915
Cell: (317) 526-6080 – **Please temporarily direct all calls to my cell phone**
Email: TDarrah@indot.in.gov

 Go Green, There is no Planet B

From: Erin Mulryan <erin@green3studio.com>
Sent: Tuesday, August 25, 2020 6:41 PM
To: Darrah, Taylor N <TDarrah@indot.IN.gov>
Subject: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination

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from unknown senders or unexpected email. ****

Hi Taylor, attached is the early coordination packet.

I have also attached the property card for the property in the SW quadrant. It appears that this establishment is a not for profit. There are trails adjacent to the building and near the bridge. Current plans don't show any impacts to the trails in the form of removal, etc. However, there may be RW take from the property- the engineer indicated that the project limits may shift south to avoid the cemetery on the north side, however I don't have updated plans yet. I wanted to bring this up now to determine if there were any 4f concerns and if so, what type of coordination is warranted.

Thank You,

Erin Mulryan, MPA

Green 3 LLC

317-634-4110

(Due to the coronavirus, I am working from home and can be reached on my cell, 317-525-1192 if needed)

green3studio.com

U.S. Department of
Homeland Security

United States
Coast Guard



Commander
Eighth Coast Guard District

1222 Spruce Street, Room 2, 102D
St. Louis, MO 63103
Staff Symbol: (dwb)
Phone: (314) 269-2381
Fax: (314) 269-2737
Rob.e.mccaskey@uscg.mil

16211
September 03, 2020

Erin Mulryan, MPA
Green 3 LLC
1104 Prospect St.
Indianapolis, IN
46203

Subj: INDOT Local Bridge improvement, DES 1600828, SR26, Salamonie River, Jay County

Dear Ms. Mulryan:

This is in response to your email dated August 25, 2020 and corresponding information requesting whether the Coast Guard will require a permit and navigational lighting for the referenced bridge project. We have examined the proposed project area with regard to its status as a navigable water of the United States for purposes of Coast Guard bridge jurisdiction.

Our examination indicates that there is no sufficient factual support for concluding that the study area, at the project location, has current or historic navigation occurring on a waterway. Since this is the case, a Coast Guard bridge permit or exemption will not be required for the referenced bridge project.

In consideration of the uses of the waterway, bridge lighting is not required.

Sincerely,

A handwritten signature in black ink, appearing to read "Eric A. Washburn", with a long horizontal line extending to the right.

ERIC A. WASHBURN
Bridge Administrator, Western Rivers
By direction of the District Commander

April 9, 2021

Erin Mulryan
Green 3, LLC
1104 Prospect Street
Indianapolis, Indiana 46203

Dear Ms. Mulryan:

The revised project to make improvements to the bridge that carries State Road 26 over Salamonie River in Jay County, Indiana (Des No. 1600828), as referred to in your letters received April 5, 2021, will cause a conversion of prime farmland.

The attached packet of information is for your use completing Parts VI and VII of the AD-1106. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICHARD
NEILSON

Digitally signed by
RICHARD NEILSON
Date: 2021.04.13
09:14:30 -04'00'

RICK NEILSON
State Soil Scientist

Enclosures



FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request				
Name of Project DES1600828_SR26_BridgeRepl_Revise		Federal Agency Involved				
Proposed Land Use transportation		County and State Jay County, Indiana				
PART II (To be completed by NRCS)		Date Request Received By NRCS 4/5/2021		Person Completing Form: JRA		
Does the site contain Prime, Unique, Statewide or Local Important Farmland? (If no, the FPPA does not apply - do not complete additional parts of this form)		YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>		Acres Irrigated 270 ac		
Major Crop(s) Corn		Farmable Land In Govt. Jurisdiction Acres: 243532 % 99		Amount of Farmland As Defined in FPPA Acres: 227482 % 93		
Name of Land Evaluation System Used LESA		Name of State or Local Site Assessment System		Date Land Evaluation Returned by NRCS 4/9/2021		
PART III (To be completed by Federal Agency)		Alternative Site Rating				
		Site A	Site B	Site C	Site D	
A. Total Acres To Be Converted Directly						
B. Total Acres To Be Converted Indirectly						
C. Total Acres In Site						
PART IV (To be completed by NRCS) Land Evaluation Information						
A. Total Acres Prime And Unique Farmland		0.37				
B. Total Acres Statewide Important or Local Important Farmland		0.00				
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		<0.001				
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		94				
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		74				
PART VI (To be completed by Federal Agency) Site Assessment Criteria (Criteria are explained in 7 CFR 558.5 b. For Corridor project use form NRCS-CPA-106)		Maximum Points	Site A	Site B	Site C	Site D
1. Area In Non-urban Use		(15)	7			
2. Perimeter In Non-urban Use		(10)	4			
3. Percent Of Site Being Farmed		(20)	0			
4. Protection Provided By State and Local Government		(20)	0			
5. Distance From Urban Built-up Area		(15)	0			
6. Distance To Urban Support Services		(15)	0			
7. Size Of Present Farm Unit Compared To Average		(10)	0			
8. Creation Of Non-farmable Farmland		(10)	0			
9. Availability Of Farm Support Services		(5)	5			
10. On-Farm Investments		(20)	0			
11. Effects Of Conversion On Farm Support Services		(10)	0			
12. Compatibility With Existing Agricultural Use		(10)	0			
TOTAL SITE ASSESSMENT POINTS		160	16	0	0	0
PART VII (To be completed by Federal Agency)						
Relative Value Of Farmland (From Part V)		100	74	0	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	16	0	0	0
TOTAL POINTS (Total of above 2 lines)		260	90	0	0	0
Site Selected: A		Date Of Selection 1/8/2021		Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>		
Reason For Selection: Meets project purpose and need with the minimal impacts to surrounding resources.						
Name of Federal agency representative completing this form: Erin Mulryan					Date: 1/8/2021	

(See Instructions on reverse side)

Form AD-1006 (03-02)

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-23009

Request Received: August 25, 2020

Requestor: Green 3 LLC
Erin Mulryan
1104 Prospect Street
Indianapolis, IN 46203

Project: SR 26 bridge (#026-38-10192; NBI #007040) rehabilitation over Salamonie River, 0.78 mile east of SR 27; Des #1600828

County/Site info: Jay

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal will require the formal approval of our agency for construction in a floodway pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application if the project does not meet the bridge exemption criteria.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Bank Stabilization & Wildlife Passage:

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. A level area of natural ground under the structure is ideal for wildlife passage. If channel clearing will result in a flat bench area above the normal water level under the structure, this area should allow wildlife passage and should remain free of riprap and other similar materials that can impair wildlife passage. If hard armoring is needed, wildlife passage can be facilitated by using a smooth-surfaced armoring material instead of riprap, such as articulated concrete block mats, fabric-formed concrete mats, or other similar smooth-surfaced material.

Minimize the use of riprap and use alternative erosion protection materials whenever possible. Where riprap must be used, we recommend placing only enough riprap to provide stream bank toe protection, such as from the toe of the bank up to the ordinary high water mark (OHWM). From the OHWM to the top of the bank, we recommend using bioengineered bank stabilization methods instead of riprap. This can provide equal or better erosion control protection than riprap. This will allow a natural, vegetated stream bank to develop and will allow wildlife passage along the creek's banks and riparian corridor. Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering

Attachments: A - Bridge Exemption Criteria

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

2) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation guidelines (and plant lists) can be found online at: <http://iac.iga.in.gov/iac/20200527-IR-312200284NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites however.

3) Nesting Birds/Roosting Bats:

Repairs to the bridge could affect any nesting birds or roosting bats. Cliff and Barn Swallows, among other species, often nest on the underside of road bridges and many bat species roost in expansion joints and other concrete crevices on road bridges. Survey the bridges for any bird nests prior to construction. Nest surveys should occur between May 7 and September 7, which denotes the main nesting season for most bird species. If nests are found with eggs, chicks, or parents actively attending to the nest (building the nest and visiting often), then repairs should be put on hold until the nests complete their nesting cycle (to fledging) or fail (by natural causes).

The Division of Fish and Wildlife (DFW) recommends bridge maintenance activities be restricted to the period between November 1 and March 1 to avoid the summer roosting period for most bats in the central part of the State. However, some endangered bats could use a bridge to roost between November and March. No matter when work is proposed, the bridge must be inspected for the presence of bats. If there is no evidence of active bat use, work can proceed. If there is evidence of active bat use, work must not occur until either the bats leave the structure for the season or a separate permit is issued to remove the bats. Please contact Linnea Petercheff (lpetercheff@dnr.in.gov) regarding permits to handle bats. If bats are present, a more formal survey to determine what species are present may be required.

The DFW recommends consulting with the State Mammologist or the US Fish and Wildlife Service before scheduling a bridge maintenance, repair, or replacement project where evidence of bat use of the structure has been observed. Information about bat use of transportation structures as well as avoidance and exclusion measures can be found at <https://www.batcon.org/pdfs/bridges/BatsBridges2.pdf> and <https://www.whitenosesyndrome.org/mmedia-education/acceptable-management-practices-for-bat-species-inhabiting-transportation-infrastructure>.

4) Stream/Wetland Habitat:

For any stream and/or wetland impacts, you may need to contact the Indiana Department of Environmental Management (IDEM) 401 program and the US Army Corps of Engineers (USACE) 404 program.

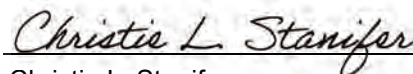
State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Eastern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not use broken concrete as riprap.
9. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
10. Minimize the movement of resuspended bottom sediment from the immediate project area.
11. Do not deposit or allow demolition/construction materials or debris to fall or otherwise enter the waterway.
12. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
13. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
14. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: September 24, 2020

Organization and Project Information

Project ID: 1600828
Des. ID: 1600828
Project Title: SR 26 over Salamonie River
Name of Organization: SJCA Inc.
Requested by: Erin Mulryan

Environmental Assessment Report

1. Geological Hazards:
 - Moderate liquefaction potential
 - Floodway
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: Low Potential
3. Active or abandoned mineral resources extraction sites:
 - None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

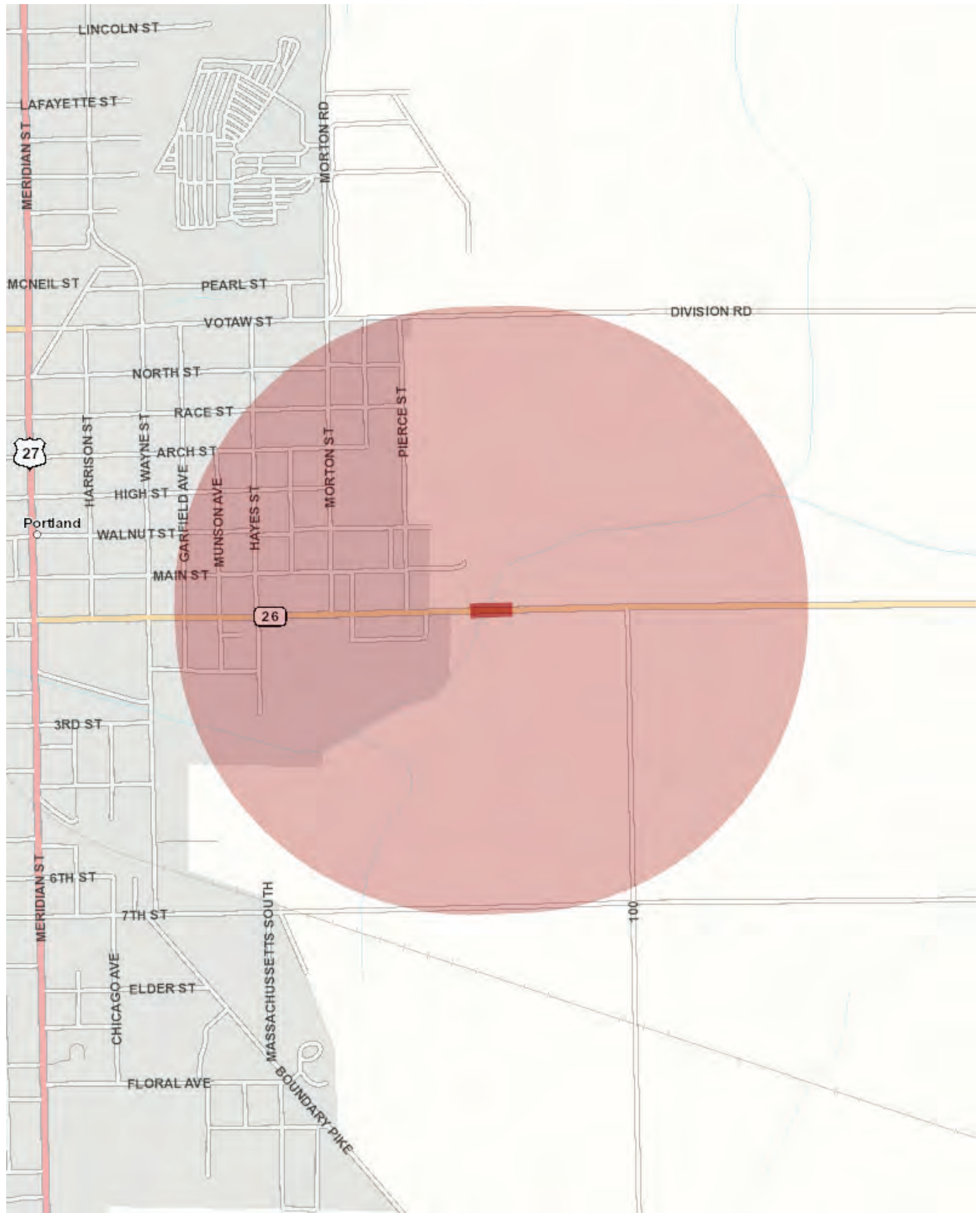
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: January 10, 2021



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Indiana Dept. of Transportation

32 South Broadway
Greenfield , IN 46140
Date

SJCA Inc.
Erin Mulryan
1104 Prospect St.
Indianapolis , IN 46203

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Replacement of Bridge No. 026-38-10192 (NBI No. 007040), a historic bridge over the Salamonie River in Portland, Jay Co. along the same general alignment. Des. No. 1600828.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the

Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - o IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - o IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - o IC 14-28-1 Flood Control Act 310 IAC 6-1
 - o IC 14-29-1 Navigable Waterways Act 312 IAC 6

- IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
- IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
- <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq> (<http://www.in.gov/idem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding

storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit:

<http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and

notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).

5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>) (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>.) New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD at adem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days

your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/ide/5284.htm> (<http://www.in.gov/ide/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

Replacement of Bridge No. 026-38-10192 (NBI No. 007040), a historic bridge over the Salamonie River in Portland, Jay Co. along the same general alignment. Des. No. 1600828.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 01/11/2021

Signature of the INDOT
Project Engineer or Other Responsible Agent 

Date: 1/11/2021

Signature of the
For Hire Consultant Erin Mulryan

Erin Mulryan

Subject: Re: [EXTERNAL] Des 1600828 SR 26 over Salamonie River early coordination
Date: Tuesday, January 12, 2021 at 12:00:46 PM Eastern Standard Time
From: Erin Mulryan
To: McCloskey, Elizabeth, jegreene@indot.in.gov, Hinkle, Meghan
Attachments: image001.png, image002.png

Hello, it is a historic bridge and the anticipated preferred alternative is replacement. Based on the last correspondence I received regarding stream impacts, they are as follows:

52.75 LF permanent to Salamonie River for new pier construction and riprap

92.4 LF temporary to Salamonie River for coffer dam and secondary containment structures on each side of the river, on the south side of the bridge. Impact plan sheets also show two sump holes on the north side of the proposed piers.

The ROW has changed. Currently 0.67 acre of re-acquisition is anticipated, with 0.06 acre of new permanent, all from the south side of SR 26. The ROW is needed for access to the bridge for demolition and construction of the new structure and for future maintenance access.

Please let me know if you need any more project info. Thanks!

Thank You,
Erin Mulryan, MPA
Director of Environmental Services

SJCA Inc.

9102 N. Meridian St, Suite 200

Indianapolis, IN 46260

317-566-0629 (Main office); 317-634-4110 (Fountain Square office)

317-566-0633 (fax)

(Due to the coronavirus, I am working from home and can be reached on my cell, 317-525-1192)

emulryan@sjcainc.com



From: Elizabeth McCloskey <elizabeth_mccloskey@fws.gov>

Date: Monday, January 11, 2021 at 9:41 AM

To: Erin Mulryan <emulryan@sjcainc.com>, "jegreene@indot.in.gov" <jegreene@indot.in.gov>, Meghan Hinkle <MHinkle@indot.IN.gov>

Subject: Re: [EXTERNAL] Des 1600828 SR 26 over Salamonie River early coordination

Good morning, is the project a rehabilitation or a replacement? Why is an additional 1.75 acres of permanent ROW needed and where will it be located? What are the anticipated impacts to the Salamonie River? Nowhere in this long letter is any of that stated. We need that information in order to provide meaningful comments on the proposed project.



United States Department of the Interior

Fish and Wildlife Service



Indiana Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

January 25, 2021

Ms. Erin Mulryan
SJCA Inc.
9102 North Meridian Street, Suite 200
Indianapolis, Indiana 46260

Project No.: Des. 1600828
Project: Bridge Replacement SR 26 over Salamonie River
Location: Portland, Jay County

Dear Ms. Mulryan:

This responds to your letter dated January 11, 2021, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The proposed project consists of the replacement of the single-span steel truss bridge, apparently with a 3-span structure of some kind; no information on the new structure is provided in your letter or the supplemental email provided on January 12th. However, the supplemental information indicated that there would be construction of new piers and the placement of riprap, plus the use of cofferdams to assist with dismantling the existing structure and building the new bridge. Although the letter of January 11th indicated that 1.75 acres of permanent right-of-way would be necessary, the supplemental information provided on the 12th stated that only 0.67 acre of re-acquisition of existing permanent right-of-way would be required, with an additional 0.06 acre of new ROW. Given the quality of the wooded riparian habitat present along the Salamonie River within the proposed project area, we hope that the permanent right-of-way required is indeed minor.

The land immediately adjacent to the SR 26 bridge is currently wooded, containing a variety of tree sizes and species. This Salamonie River corridor provides valuable habitat for wildlife. Therefore, we request that the loss of riparian trees be mitigated as close to the project impact

site as possible, preferably along the Salamonie River or a nearby tributary. We support the woodland mitigation guidelines of the Indiana Department of Natural Resources contained in their Information Bulletin #17 (<http://www.in.gov/legislative/iac/20200527-IR-312200284NRA.xml.pdf>) which states that the mitigation ratio for non-wetland floodway forest losses of more than 1 acre is to be 2:1 (2 acres replanted for every acre destroyed), planted as close to the impact site as possible; loss of less than an acre is to be mitigated at a 1:1 ratio or 5 trees for each lost tree of 10 inches dbh or greater. If any of the woodland that would be removed is forested wetland, the mitigation ratio is 4:1. This tree replacement requirement is not related to any possible mitigation needed for potential impact to the Indiana bat or northern long-eared bat under the range-wide programmatic informal consultation process.

We may have additional comments about the proposed work within the Salamonie River under the Clean Water Act permit process.

ENDANGERED SPECIES

The proposed project is within the range of the Federally endangered Indiana bat (*Myotis sodalis*) and the threatened northern long-eared bat (*Myotis septentrionalis*). The impacts to the 2 bat species will be evaluated utilizing the Section 7 Range-wide Programmatic Consultation process.

We appreciate the opportunity to comment on this proposed project. Please keep us informed as project planning progresses. For further discussion, please contact Elizabeth McCloskey at elizabeth_mccloskey@fws.gov.

Sincerely yours,

/s/ *Elizabeth S. McCloskey*

for Scott E. Pruitt
Supervisor

Sent via email January 25, 2021; no hard copy to follow.

cc: Christie Stanifer, Environmental Coordinator, Division of Fish and Wildlife, Indianapolis, IN

Subject: Re: Portland ADA Plan
Date: Wednesday, January 27, 2021 at 2:16:15 PM Eastern Standard Time
From: Lori Phillips
To: Erin Mulryan
Attachments: image001.png, image002.png

Hi Erin,

Unfortunately, this is not online. The Resolution was passed by the City of Portland Common Council on January 16, 2017.

On Wed, Jan 27, 2021 at 12:57 PM Erin Mulryan <emulryan@sjcainc.com> wrote:

Hello, is this plan available online? I'm working on the environmental document for the SR 26 over Salamonie River project and one thing we need to include in the document is a discussion regarding the local ADA plan. I mostly need to know when it was adopted.

I have also attached the project early coordination packet for reference.

Thank You,

Erin Mulryan, MPA

Director of Environmental Services

SJCA Inc.

9102 N. Meridian St, Suite 200

Indianapolis, IN 46260

317-566-0629 (Main office); 317-634-4110 (Fountain Square office)

317-566-0633 (fax)

(Due to the coronavirus, I am working from home and can be reached on my cell, 317-525-1192)

emulryan@sjcainc.com



From: Lori Phillips <cityclerktreas@thecityofportland.net>
Date: Tuesday, January 26, 2021 at 8:44 AM
To: Erin Mulryan <emulryan@sjcainc.com>
Subject: Re: Portland ADA Plan

Erin,

Yes, we do have an ADA plan on file for the following Portland public facilities:

Street Department

Swimming pool

Airport

Portland City Hall / Police Dept.

Community Resource Center

Fire Station

Haynes Park, Milton Miller, Hudson and Wilson Parks

Also, Portland ADA ramps.

On Tue, Jan 26, 2021 at 7:52 AM Erin Mulryan <emulryan@sjcainc.com> wrote:

Hello, the zoning department gave me your contact information. I'm inquiring about whether the City of Portland has an ADA plan on file? I found the ADA plan for Jay Co but didn't find one for the city. Your help is greatly appreciated!

Thank You,

Erin Mulryan, MPA

Director of Environmental Services

SJCA Inc.

9102 N. Meridian St, Suite 200

Indianapolis, IN 46260

Subject: Re: Des 1600828 SR 26 over Salamonie River early coordination
Date: Monday, February 22, 2021 at 2:16:04 PM Eastern Standard Time
From: Tri-State
To: Erin Mulryan

Our big show is in August and our big Swap Meet is in May so we really don't have any concerns since the construction will be from October to April.

Thanks
Chris Englehardt

Sent from Chris Englehardt's

iPhone

On Feb 20, 2021, at 3:08 PM, Erin Mulryan <emulryan@sjcainc.com> wrote:

Hello, I'm following up on this email and whether the Tri-State Tractor Show committee has any comments or concerns regarding the bridge replacement project. Thanks!

From: Erin Mulryan <emulryan@sjcainc.com>
Date: Friday, January 22, 2021 at 3:41 PM
To: "tristategasengine@gmail.com" <tristategasengine@gmail.com>
Subject: Des 1600828 SR 26 over Salamonie River early coordination

Hello, I'm working with the Indiana Dept of Transportation (INDOT) on the above-mentioned project in Portland, Indiana. It is currently slated for construction for October 2022 to April 2023 and will require a road closure and detour. I'm providing you with this information to get input on potential impacts to the Tri-State Gas Engine and Tractor Association Show because I know that event is very big and attracts visitors far and wide. FYI, I also submitted an inquiry through your show website.

If you would like to provide comments or concerns regarding the project, please contact me via email, mail, or phone. Your response is kindly requested within 30 days. Thank you!

Thank You,
Erin Mulryan, MPA
Director of Environmental Services
SJCA Inc.

9102 N. Meridian St, Suite 200
Indianapolis, IN 46260
317-566-0629 (Main office); 317-634-4110 (Fountain Square office)
317-566-0633 (fax)

(Due to the coronavirus, I am working from home and can be reached on my cell, 317-525-1192)

emulryan@sjcainc.com

<image001.png>

Subject: RE: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination
Date: Tuesday, May 18, 2021 at 5:34:50 AM Pacific Daylight Time
From: Courtade, Julian
To: Erin Mulryan
Attachments: image008.png, image009.png, image010.png, image011.png, image012.png, image013.png, image014.png, image015.png, image016.png, image017.png, image018.png, image019.png, image020.png, image021.png, image022.png, image023.png, image024.png, image025.png, image026.png, image027.png, image028.png, image029.png, image030.png, image031.png, image032.png, image033.png, image034.png, image035.png, image036.png

Hi Erin –

Let me forward your email to our tall structure coordinator. He is currently out of town but should be back next Monday. For now, I would continue to submit your project to the FAA for airspacing. Let me know if you need anything else in the meantime!

Best,

Julian L. Courtade

Chief Airport Inspector

100 North Senate Ave, N758-MM

Indianapolis, IN 46204

Cell: (317) 954-7385

Email: jcourtade@indot.in.gov



From: Erin Mulryan <emulryan@sjcainc.com>
Sent: Monday, May 17, 2021 9:02 AM
To: Courtade, Julian <JCourtade@indot.IN.gov>
Subject: Re: Des 1600828, SR 26 over Salamonie River, Jay Co. early coordination

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

May 17, 2021

Consultation Code: 03E12000-2021-SLI-0379

Event Code: 03E12000-2021-E-06027

Project Name: Des. No 1600828, SR 26 over Salamonie River, Jay Co.

Subject: Updated list of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

Project Summary

Consultation Code: 03E12000-2021-SLI-0379

Event Code: 03E12000-2021-E-06027

Project Name: Des. No 1600828, SR 26 over Salamonie River, Jay Co.

Project Type: TRANSPORTATION

Project Description: This historic bridge project is in Wayne Township, Jay County, Indiana. The project is located on SR 26 and involves INDOT Bridge No. 026-38-03430A (NBI 007040) on SR 26 over Salamonie River, 0.78 mile east of US 27, on the east side of the City of Portland. INDOT Bridge No. 026-38-03430A is a single span, steel Parker through truss structure built in 1941 and has been determined eligible for the National Register of Historic Places. The existing structure is 150 feet in length with a 28-foot-wide clear roadway width and two-foot-wide shoulders on a zero-degree skew. The bridge has a cast-in-place deck with non-standard steel bridge railings upon concrete abutments on spread footings. The structure carries two 11-foot-wide travel lanes over Salamonie Creek. The need for this project is due to the existing bridge not meeting current INDOT design criteria for capacity or shoulder width. Currently, the proposed preferred alternative is replacement, with construction of a new bridge on essentially the same alignment as existing. The new bridge would consist of three spans at 70 feet each to provide adequate hydraulic capacity for the crossing. The typical section would consist of two 11-foot wide travel lanes with 4-foot wide shoulders for a clear travel way of 30 feet. The out-to-out measurement of the bridge deck will be 38 feet, 10 inches. A 6-foot, 10-inch-wide sidewalk will be added on the south side of the bridge. Two wall piers and end bents would support the structure. Four pipes are also located within the project area that convey stormwater under farmland access drives and driveways. No work will occur to these structures; they will remain in place. No temporary or permanent lighting is proposed. The maintenance of traffic during construction of the preferred alternative will involve road closure and a detour that will utilize SR 49, SR 67, and US 27/SR 67. Project letting is anticipated for April 2022. Land use in the vicinity of the project includes forested riparian corridor adjacent to the bridge structure, with light commercial and residential development to the west and agricultural land to the east. The project area consists of riparian forest surrounding the Salamonie River consisting of ash-leaf maple, black walnut, common hackberry, boxelder, mulberry, giant goldenrod, Indian hemp, and reed canary grass. A review of the USFWS database completed on October 21, 2019 did not indicate the presence of endangered bat species in or within the 0.5-mile search radius of the project area. A site inspection on August 19, 2020 did not find evidence of the presence of bats. There is suitable habitat along the riparian corridor. Six (6) trees over 10 inches in DBH and approximately 0.12 acre of small trees will be removed, and

approximately 0.75 acre of vegetation removal will be needed to access the existing structure for demolition and construction of the new bridge. Approximately 0.73 acre of permanent right-of-way will be required.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.43259399180788,-84.96298093199493,14z>



Counties: Jay County, Indiana

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none">▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

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Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

January 15, 2021

Consultation code: 03E12000-2021-I-0379

Event Code: 03E12000-2021-E-02561

Project Name: Des. No 1600828, SR 26 over Salamonie River, Jay Co.

Subject: Concurrence verification letter for the 'Des. No 1600828, SR 26 over Salamonie River, Jay Co.' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des. No 1600828, SR 26 over Salamonie River, Jay Co.** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. No 1600828, SR 26 over Salamonie River, Jay Co.

Description

This historic bridge project is in Wayne Township, Jay County, Indiana. The project is located on SR 26 and involves INDOT Bridge No. 026-38-03430A (NBI 007040) on SR 26 over Salamonie River, 0.78 mile east of US 27, on the east side of the City of Portland. INDOT Bridge No. 026-38-03430A is a single span, steel Parker through truss structure built in 1941 and has been determined eligible for the National Register of Historic Places. The existing structure is 150 feet in length with a 28-foot-wide clear roadway width and two-foot-wide shoulders on a zero-degree skew. The bridge has a cast-in-place deck with non-standard steel bridge railings upon concrete abutments on spread footings. The structure carries two 11-foot-wide travel lanes over Salamonie Creek. The need for this project is due to the existing bridge not meeting current INDOT design criteria for capacity or shoulder width. Currently, the proposed preferred alternative is replacement, with construction of a new bridge on essentially the same alignment as existing. The new bridge would consist of three spans at 70 feet each to provide adequate hydraulic capacity for the crossing. The typical section would consist of two 11-foot wide travel lanes with 4-foot wide shoulders for a clear travel way of 30 feet. The out-to-out measurement of the bridge deck will be 38 feet, 10 inches. A 6-foot, 10-inch-wide sidewalk will be added on the south side of the bridge. Two wall piers and end bents would support the structure. Four pipes are also located within the project area that convey stormwater under farmland access drives and driveways. No work will occur to these structures; they will remain in place. No temporary or permanent lighting is proposed. The maintenance of traffic during construction of the preferred alternative will involve road closure and a detour that will utilize SR 49, SR 67, and US 27/SR 67. Project letting is anticipated for April 2022. Land use in the vicinity of the project includes forested riparian corridor adjacent to the bridge structure, with light commercial and residential development to the west and agricultural land to the east. The project area consists of riparian forest surrounding the Salamonie River consisting of ash-leaf maple, black walnut, common hackberry, boxelder, mulberry, giant goldenrod, Indian hemp, and reed canary grass. A review of the USFWS database completed on October 21, 2019 did not indicate the presence of endangered bat species in or within the 0.5-mile search radius of the project area. A site inspection on August 19, 2020 did not find evidence of the presence of bats. There is suitable habitat along the riparian corridor. Six (6) trees over 10 inches in DBH and approximately 0.12 acre of small trees will be removed, and approximately 0.75 acre of vegetation removal will be needed to access the existing structure for demolition and construction of the new bridge. Approximately 0.73 acre of permanent right-of-way will be required.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Bat Inspection Form Des 1600828 8.19.2020.pdf* <https://ecos.fws.gov/ipac/project/6HQI3TQAYZHU3MFY4Z6WBZLKAI/projectDocuments/24748154>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

No

31. Will the project install new or replace existing **permanent** lighting?

No

32. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

33. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

34. Will the project raise the road profile **above the tree canopy**?

No

35. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

36. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

39. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

40. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

41. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

42. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.12

4. Please describe the proposed bridge work:
Replacement of SR 26 bridge over Salamonie River
5. Please state the timing of all proposed bridge work:
anticipated 10/2022 to 4/2023
6. Please enter the date of the bridge assessment:
8/19/2020

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 29, 2020. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 08/19/2020 Time of Inspection: 11:00 am	Initial Inspection <input checked="" type="checkbox"/> Follow-up Inspection <input type="checkbox"/> Construction <input type="checkbox"/>	Temp: 73° F Wind: none Precip: none Sunrise: 7:02 am Sunset: 8:32 pm
County: Jay	Inspected by: Erin Mulryan, Laura Rogers	
GPS Northing: 40°25'57.2"N Easting: 84°57'48.5"W UTM Zone: 16 S	Contract Number: B 39818 Des 1600828	Anticipated Start Date for Construction: 2023

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: SR 26, Salamonie River	Station: 56+60.00
Bridge/Culvert number: 026-38-10192	Number of Spans: 1
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input checked="" type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input checked="" type="checkbox"/> Other (describe) historic Parker truss
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): NA
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard?	
No bats observed	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining N/A	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified:

Subject: RE: IPAC for Des 1600828, SR 26 over Salamonie River
Date: Friday, January 15, 2021 at 10:56:12 AM Eastern Standard Time
From: Darrah, Taylor N
To: Erin Mulryan
Attachments: image009.png, image010.png, image011.png, image012.png, image013.png, image014.png, image016.png, image017.png, image018.png, image019.png, image020.png, image021.png, image001.png

Great new! IPaC has updated and unlocked the project for me. INDOT reviewed the determination and concurs. The determination was submitted to the USFWS on January 15, 2021 for their 14-day review period. The concurrence verification letter needed for permit applications is now available in IPaC.

Thank you,

Taylor Darrah

Environmental Section Manager

Indiana Department of Transportation

32 South Broadway

Greenfield, IN 46140

Office: (317) 467-3915

Cell: (317) 526-6080

Email: TDarrah@indot.in.gov



From: Erin Mulryan <emulryan@sjcainc.com>
Sent: Monday, December 28, 2020 5:01 PM
To: Darrah, Taylor N <TDarrah@indot.IN.gov>
Subject: Re: IPAC for Des 1600828, SR 26 over Salamonie River

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Appendix D
Section 106 of NHPA

Form Prescribed by State Board of Accounts
Ind Dept of Transportation
Public Notice Des No 1600828

(Governmental Unit)

Jay County, Indiana

General Form No. 99P (Revised 2009A)
The Commercial Review
To: The Graphic Printing Co., Inc.

P.O. Box 1049
Portland, IN 47371

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall total more than four solid lines of type in which the body of the advertisement is set) – number of equivalent lines	3
Head – number of lines	2
Body – number of lines	54
Tail – number of lines	4
Total number of lines in notice.....	63

COMPUTATION OF CHARGES

63 lines, 2 columns wide equals 126 equivalent lines	
at 0.4346 cents per line	\$ 54.76
Additional charge for notices containing rule or tabular work (50 percent of above amount)	
Charge for extra proofs of publication (\$1.00 for each proof in excess of two)	
TOTAL AMOUNT OF CLAIM.....	\$ 54.76 Paid

DATA FOR COMPUTING COST

Width of single column in picas: 8.5	Size of type 7 point
Number of insertions 1	

Pursuant to the provisions and penalties of IC 5-11-10-1,

I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 times. The dates of publication being as follows:

March 6, 2021

Additionally, the statement checked below is true and correct:

..... Newspaper does not have a website.

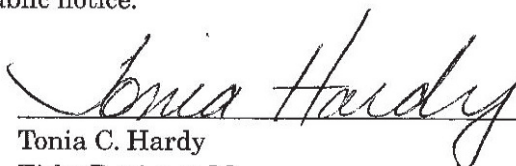
X..... Newspaper has a website and this public notice was posted on the same day as it was published in the newspaper.

..... Newspaper has a website, but due to technical problem or error, public notice was posted on.....

..... Newspaper has a website but refuses to post the public notice.

March 8, 2021

Date: _____


Tonia C. Hardy
Title: Business Manager

Public Notice

Public Notice
Des. No. 1600828

The Indiana Department of Transportation (INDOT) is planning to undertake a historic bridge project funded in part by the Federal Highway Administration (FHWA). The project is located on State Road (SR) 26 over the Salamonie River, 0.75 mile east of SR 27, in Portland, Jay County, Indiana.

Under the preferred alternative, the proposed project would involve removing the existing Bridge No. 026-38-03430A (NBI 7040), a 150-foot-long single span steel Parker through-truss built in 1941 and constructing a new bridge on essentially the same alignment as existing. The new bridge would consist of three spans at 50', 100' and 50' to provide adequate hydraulic capacity for the crossing. The typical section would consist of two 11'-0" travel lanes with 4'-0" shoulders for a clear travel way of 30'-0". Bridge railing would be type FC concrete barriers. The out-to-out measurement of the bridge deck would be 33'-0". Two wall piers and end bents would support the structure. It is anticipated the project will require a total right-of-way acquisition of 0.73 acre. No relocation of residents or businesses will be required for this project.

Bridge No. 026-38-03430A, which is eligible for the National Register of Historic Places under Criterion C for its engineering significance, is classified as a "Non-Select" bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B. of the Historic Bridges Programmatic Agreement (HBPA) will be followed to fulfill FHWA's Section 106 responsibilities for the project. Per Stipulation III.B. of the HBPA, INDOT will hold a public hearing for the project prior to completion of National Environmental Policy Act (NEPA) studies. The hearing will be advertised at a later date.

To satisfy FHWA's Section 106 responsibilities for the other historic resources that may be in the Area of Potential Effects (APE), FHWA will comply with the requirements of 36 CFR Parts 800.3-800.6. Regarding other resources in the APE, INDOT, on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800.11(d) is available for inspection in the office of SJCA Inc. (formerly Green 3, LLC). Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Scott Henley, SJCA Inc. 1104 Prospect Street, Indianapolis, IN 46203 317.566-0629 or shenley@sjcainc.com no later than April 7, 2021.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Jeremy Greene, INDOT Project Manager, (317) 467-3472 or jegreene@indot.IN.gov to the Instructions to Bidders that are included in the Bidding Documents.

SJCA, Inc.
1104 Prospect St.,
Indianapolis, IN 46203
317-566-0629

CR 3-6-2021 HSPAXLP

Public Notice
Des. No. 1600828

The Indiana Department of Transportation (INDOT) is planning to undertake a historic bridge project funded in part by the Federal Highway Administration (FHWA). The project is located on State Road (SR) 26 over the Salamonie River, 0.75 mile east of SR 27, in Portland, Jay County, Indiana.

Under the preferred alternative, the proposed project would involve removing the existing Bridge No. 026-38-03430A (NBI 7040), a 150-foot-long single span steel Parker through-truss built in 1941 and constructing a new bridge on essentially the same alignment as existing. The new bridge would consist of three spans at 50', 100' and 50' to provide adequate hydraulic capacity for the crossing. The typical section would consist of two 11'-0" travel lanes with 4'-0" shoulders for a clear travel way of 30'-0". Bridge railing would be type FC concrete barriers. The out-to-out measurement of the bridge deck would be 33'-0". Two wall piers and end bents would support the structure. It is anticipated the project will require a total right-of-way acquisition of 0.73 acre. No relocation of residents or businesses will be required for this project.

Bridge No. 026-38-03430A, which is eligible for the National Register of Historic Places under Criterion C for its engineering significance, is classified as a "Non-Select" bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B. of the Historic Bridges Programmatic Agreement (HBPA) will be followed to fulfill FHWA's Section 106 responsibilities for the project. Per Stipulation III.B. of the HBPA, INDOT will hold a public hearing for the project prior to completion of National Environmental Policy Act (NEPA) studies. The hearing will be advertised at a later date.

To satisfy FHWA's Section 106 responsibilities for the other historic resources that may be in the Area of Potential Effects (APE), FHWA will comply with the requirements of 36 CFR Parts 800.3-800.6. Regarding other resources in the APE, INDOT, on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800.11(d) is available for inspection in the office of SJCA Inc. (formerly Green 3, LLC). Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Scott Henley, SJCA Inc. 1104 Prospect Street, Indianapolis, IN 46203 317.566-0629 or shenley@sjcainc.com no later than April 7, 2021.

In accordance with the "Americans with Disabilities Act", if you have a disability for which INDOT needs to provide accessibility to the document(s) such as interpreters or readers, please contact Jeremy Greene, INDOT Project Manager, (317) 467-3472 or jegreene@indot.IN.gov.

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



March 25, 2021

Scott Henley
SJCA Inc.
1104 Prospect Street
Indianapolis, Indiana 46203

State Agency: Indiana Department of Transportation (“INDOT”)
Federal Agency: Federal Highway Administration, Indiana Division (“FHWA”)

Re: DUAL REVIEW: Indiana Department of Transportation’s finding of “No historic properties affected” on behalf of the Federal Highway Administration for the SR 26 over Salamonie River, Bridge No. 026-38-03430A (NBI 007040) Project (Des. No. 1600828; DHPA No. 24076)

Dear Mr. Henley:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, the Section 106 “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (“Indiana Historic Bridges PA”), Indiana Code 14-21-1-18, and 312 Indiana Administrative Code (“IAC”) 20-4, the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your March 3, 2021 submission which enclosed INDOT’s finding and supporting documentation for the aforementioned project in Portland, Wayne Township, Jay County, Indiana.

For the benefit of the Indiana Historic Preservation Review Board (“Review Board”) members and other recipients of this letter who are not Section 106 consulting parties, please be aware that documents submitted for review of this project can be found online at INSCOPE (<http://www.erms.indot.in.gov/Section106Documents/>). From there, search by this project’s designation number: 1600828.

As previously indicated, we agree that the subject bridge carrying SR 26 over the Salamonie River (Bridge 026-38-03430A, NBI 007040) is the only above-ground historic property eligible for inclusion in the National Register of Historic Places (“NRHP”) located within the project’s area of potential effects. As a “Non-Select” bridge, FHWA will follow the Project Development Process outlined in Stipulation III.B of the Historic Bridges PA to satisfy Section 106 responsibilities for this project. We agree that there are no other historic properties located within the project’s area of potential effects listed or eligible for inclusion in the NRHP.

Additionally, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we concur with the opinion of the archaeologist, as expressed in the Phase Ia archaeological literature review and field reconnaissance survey report (Jackson, 01/06/2021), that Site 12-Ja-0700 (which was identified during these archaeological investigations) does not appear eligible for inclusion in the NRHP.

Furthermore, it is our understanding that an unnamed cemetery is extant immediately just north of the northwesternmost portions of the proposed project area, that data obtained from previous INDOT-CRO investigations determined that it is highly likely that graves are present in the cemetery, and that it was recommended that all project-related ground-disturbing activities should avoid the cemetery. It is our understanding that cemetery development plan for this cemetery was developed in 2009, and that an updated cemetery development plan for this cemetery will be developed.

As a reminder, if any portion of the proposed project area is within 100 feet of a cemetery, then a cemetery development plan may be necessary under IC 14-21-1-26.5. The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

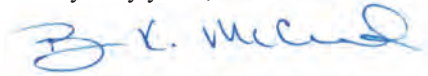
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Accordingly, we concur with INDOT's March 3, 2021 Section 106 finding of "no historic properties affected" on behalf of FHWA, for this federal undertaking.

The structures reviewer on the Indiana SHPO staff for this project is Danielle Kauffmann, and the archaeological reviewer is Wade T. Tharp. However, if you have a question about our comments or about the review process, we ask that you initially direct your question to a staff member of the INDOT Cultural Resources Office who has been assigned to this project.

In any future correspondence regarding the bridge project that carries SR 26 over the Salamonie River in Portland, Jay County, Indiana (Des. No. 1600828), please continue to refer to DHPA No. 24076.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

EMC to federal and state agency or consultant staff members:

Robert Dirks, PE, FHWA
Anuradha Kumar, INDOT
Mary Kennedy, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Scott Henley, SJCA Inc.
Danielle Kauffmann, DNR-DHPA
Wade T. Tharp, DNR-DHPA

EMC to Indiana Historic Preservation Review Board Members:

J. Scott Keller, Review Board
Anne Shaw Kingery, Review Board
Daniel Kloc, AIA, Review Board
Jason Larrison, AIA, Review Board
Chandler Lighty, Review Board
Beth K. McCord, DNR-DHPA, Review Board
April Sievert, Ph.D., Review Board
Christopher Smith, Deputy Director, DNR, and Chairman, Review Board

EMC to potentially interested persons:

Delaware Tribe of Indians
Eastern Shawnee Tribe of Oklahoma
Forest County Potawatomi Community
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians

The Honorable John W. Boggs, Mayor, City of Portland
And member, Portland Historic Preservation Commission
Ami Huffman, Portland Historic Preservation Commission
Portland Street Department
Chad Aker, Jay County Commissioner
Richard L. Huffman, Jay County Commissioner
Mike Leonhard, Jay County Commissioner
Dan Watson, Jay County Highway Department
Donnie Corn, Jay County Highway Department
Rob Weaver, WPGW Radio
Jane Spencer, Jay County Historian
Larry Hiatt & Kay Locker, Jay County Historical Society
Jessie Russet, Indiana Landmarks, Eastern Regional Office
James L. Cooper, Ph.D., Professor Emeritus of History, DePauw University
Paul Brandenburg, Indiana Historic Spans Task Force
Nathan Holth, historicbridges.org
Tony Dillon, Historic Hoosier Bridges
Kitty Henderson, Historic Bridge Foundation



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (317) 296-0799

Eric Holcomb, Governor
Joe McGuinness, Commissioner

March 3, 2021

This letter was sent to the listed parties.

RE: Dual Review Project: SR 26 over Salamonie River, Bridge 026-38-03430 A (NBI 007040) Project, Jay County, Des. No. 1600828, DHPA No. 24076

Dear Consulting Party,

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), proposes to proceed with the SR 26 over Salamonie River, Bridge No. 026-38-03430A (NBI 007040) Project, Des. No. 1600828.

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on June 28, 2019. In addition, a letter distributed on March 3, 2020 notified consulting parties that a Historic Bridge alternatives Analysis was available for review and comment. In addition, a letter distributed on November 5, 2020 notified consulting parties that a historic property report was available for review and comment. A letter distributed on January 7, 2021 notified consulting parties that an archaeology report was available for review and comment (tribes only).

The proposed undertaking is on State Road (SR) 26 over the Salamonie River, located 0.75 mile east of SR 27, in Portland, Jay County, Indiana. It is within Wayne Township, Portland USGS Quadrangle in Section 21, Township 23 North, Range 14 East.

The purpose of this bridge project is to restore the crossing of SR 26 over Salamonie River to a satisfactory condition and increase the safe carrying capacity of the bridge from the current 28 tons to 36 tons. The need for the project is that the existing bridge does not meet current INDOT design criteria for capacity or shoulder width.

Bridge No. 026-38-03430 A (NBI 007040) is a 150-foot-long one-span steel parker through truss built in 1941. The bridge has a clear roadway width of 28 ft. on a zero-degree skew, featuring a concrete cast-in-place deck with non-standard steel bridge railings upon concrete abutments on spread footings. The bridge carries SR 26, which consists of two 11 ft. travel lanes complete with two-foot-wide shoulders each and six-inch by six-inch concrete curbs.

Based on the results of the Historic Bridge Alternatives Analysis (HBAA), if no party elects to take responsibility of the existing structure, the preferred alternative is Alternative F: Replacement – Demolition of Historic Bridge and New Bridge Construction. This alternative proposes to remove the existing bridge and construct a new bridge on essentially the same alignment as existing. The new bridge would consist of three spans at 50', 100' and 50' to provide adequate hydraulic capacity for the crossing. The typical section would consist of two 11'-0" travel lanes with 4'-0" shoulders for a clear travel way of 30'-0". Bridge railing would be type FC concrete barriers. The out-to-out measurement of the bridge deck would be 33'-0". Two wall piers and end bents would support the structure.

This project is currently scheduled for letting in April 2022. It was anticipated that the project will require a total right-of-way acquisition of 1.75 acres; currently it is anticipated that only 0.73 acre will be required. Due to right-of-way research, one parcel of 0.67 acre will be re-acquisition, and the other 0.06 acre will be permanent. No relocations of residents or businesses will be required for this project.

SJCA Inc. (formerly Green 3, LLC) is under contract with INDOT to advance the environmental documentation for the referenced project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because 026-38-03430A (NBI 007040) is a "Non-Select" bridge, the procedures outlined in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Please note that, per the permanent rule issued by the Indiana Department of Natural Resources effective August 14, 2013 (312 IAC 20-4-11.5), INDOT is requesting that this project be subjected to "dual review"; that is, reviewed by the Division of Historic Preservation and Archaeology simultaneously under 54 U.S.C. 306108 (Section 106) and IC 14-21-1-18 (Indiana Preservation and Archaeology Law dealing with alterations of historic sites and structures requiring a Certificate of Approval). Pursuant to Section 11.5(f) of this rule, at the conclusion of the review process we anticipate that the Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval under IC 14-21-1-18.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, Bridge No. 026-38-03430A (NBI 7040) is recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified one site within the project area. As a result of these efforts, site 12-Ja-700 was recommended not eligible for listing in the NRHP and no further work was recommended.

In a letter dated February 8, 2021, SHPO staff responded to the archaeological report and concurred with the findings of the archaeology report stating, "we concur with the opinion of the archaeologist, as expressed in the Phase Ia archaeological literature review and field reconnaissance survey report (Jackson, 01/06/2021), that Site 12-Ja-0700 (which was identified during these archaeological investigations) does not appear eligible for inclusion in the NRHP."

SHPO staff also commented "Furthermore, it is our understanding that an unnamed cemetery is extant immediately just north of the northwesternmost portions of the proposed project area, that data obtained from previous INDOT-CRO investigations determined that it is highly likely that graves are present in the cemetery, and that it was recommended that all project-related ground-disturbing activities should avoid the cemetery. It is our understanding that cemetery development plan for this cemetery was developed in 2009, and that an updated cemetery development plan for this cemetery will be developed." A cemetery development plan is planned for this project.

The effects finding and 800.11(d) documentation are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request as soon as you can.

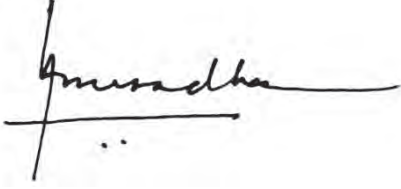
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience.

For questions concerning specific project details, you may contact Scott Henley of SJCA Inc. at (317) 566-0629 or shenley@sjcainc.com. All future responses regarding the proposed project should be forwarded to SJCA Inc. at the following address:

Scott Henley
Cultural Resources Associate
SJCA Inc.
1104 Prospect Street
Indianapolis, IN 46203
shenley@sjcainc.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-416-0876 or Kari Carmany-George at FHWA at K.CarmanyGeorge@dot.gov or 317-226-5629.

Sincerely,

A handwritten signature in black ink, appearing to read "Anuradha", with a horizontal line drawn underneath it.

Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:
800.11 documentation (available via IN SCOPE)

Distribution List:

Indiana State Historic Preservation Officer, dkauffmann@dnr.in.gov, wtharp1@dnr.in.gov

Indiana Landmarks Eastern Regional Office, jrussett@indianalandmarks.org

Rob Weaver, rob.weaver@wpgwradio.us

Miami Tribe of Oklahoma

SR 26 over Salamonie River, Bridge No. 026-38-03430A (NBI 7040) Project

800.11(d) DOCUMENTATION AND EFFECTS FINDING

Portland, Wayne Township, Jay County, Indiana
Des. No. 1600828; DHPA No. 24076

March 2021



Prepared for:
USI Consultants, Inc.
8415 E 56th Street
Indianapolis, IN 46216

By:

Karen Wood
Environmental and Cultural Resource Manager
SJCA, Inc. (Green 3, LLC)
Historic Fountain Square
1104 Prospect Street
Indianapolis, IN
46203



**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING**

**SR 26 over Salamonie River, Bridge No. 026-38-03430A (NBI 7040) Project
DES. NO.: 1600828; DHPA No. 24076**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

The Area of Potential Effects (APE) of the project includes all properties within or adjacent to the project and those with a proximate viewshed of the project. The APE extends approximately 0.10-mile at its widest point and extends 0.05-mile at its longest point. See map of APE in Appendix A.

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

The APE contains one property eligible for listing in the National Register of Historic Places (NRHP): Bridge No. 026-38-03430A (NBI 7040), a 150-foot Parker through-truss design with additionally braced design for a wide deck built in 1941. The bridge is eligible under Criterion C as it represents an early or distinctive phase in bridge construction. The Indiana State Highway Commission significantly modified a standard 150-foot Parker through-truss design in the late 1930s to accommodate roadways wider than 25 feet. The bridge is important as one of six or fewer examples within a district of the Indiana Department of Transportation. Further, it retains the historic integrity necessary to convey its engineering significance. The bridge is classified as "Non-Select." No other properties within the APE are listed in or recommended eligible for listing in the NRHP.

EFFECT FINDING

Bridge No. 026-38-03430A (NBI 7040): Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration—Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Bridge No. 026-38-03430A (NBI 7040) has been classified as a Non-Select Bridge by the INDOT Historic Bridge Inventory and, thus, the procedures outlined in Stipulation III.B of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge.

Therefore, the finding for this project only applies to other resources located within the APE and not Bridge No. 026-38-03430A (NBI 7040). This document will satisfy the Section 106 responsibilities for other resources located in the APE. Regarding other resources located in the APE, INDOT, acting on FHWA's behalf, has determined a "No historic properties affected" finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Bridge 026-38-03430A (NBI 7040) - This resource is used for transportation purposes. Bridge No. 026-38-03430A (NBI 7040) will be evaluated through the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges*.

Anuradha V. Kumar

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

03/03/2021

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)
SR 26 over Salamonie River, Bridge No. 026-38-03430A (NBI 7040) Project
DES. NO.: 1600828; DHPA No. 24076**

1. DESCRIPTION OF THE UNDERTAKING

The Indiana Department of Transportation (INDOT), with funding from the Federal Highway Administration (FHWA), plans to proceed with the SR 26 over Salamonie River, Bridge No. 026-38-03430A (NBI 7040) Project, Des. No. 1600828. The project is located on State Road (SR 26) over the Salamonie River, located 0.75 mile east of SR 27, in Portland, Jay county. Specifically, the project is within Wayne Township, Portland USGS Quadrangle in Section 21, Township 23 North, Range 14 East.

The purpose of this bridge project is to restore the crossing of SR 26 over Salamonie River to a satisfactory condition and increase the safe carrying capacity of the bridge from the current 28 tons to 36 tons. The need for the project is that the existing bridge does not meet current INDOT design criteria for capacity or shoulder width.

Bridge No. 026-38-03430A (NBI 7040) is a 150-foot-long single span steel Parker through-truss built in 1941. It was given “Non-Select” status in the Historic Bridge Inventory Report (HBIR) and was determined eligible for listing in the NRHP.

The APE of the project includes all properties within or adjacent to the project and those with a proximate viewshed of the project. The APE consists of forested areas, agricultural fields, and commercial and residential properties. The APE extends approximately 0.10-mile at its widest point and extends 0.05-mile at its longest point. See map of APE in Appendix A.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The NRHP, Indiana Register of Historic Sites and Structures (State Register), the *Jay County Interim Report* (1985), the State Historic Architectural and Archaeological Research Databases (SHAARD and the Indiana Historic Buildings, Bridges and Cemeteries Map) were consulted. The APE contains no resources listed in the NRHP. Bridge No. 026-38-03430A (NBI 7040) is listed as Historic Bridge (HB) – 0824. The INDOT-sponsored *Indiana Historic Bridge Inventory* (February 2009) by M&H Architecture, Inc. was also reviewed. Bridge No. 026-38-03430A (NBI 7040) is listed as “eligible” and classified as “Non-Select.” Research indicated that one previous investigation had occurred within the project APE. In 2008, INDOT Cultural Resources staff conducted an above-ground properties investigation for the SR 26 pavement Replacement Project (Des. No. 0100715) (Kumar 2009). As a result of that investigation, one property, Bridge No. 026-38-03430A (NBI 7040), was recommended eligible under Criterion C. There are no HABS/HAER/HALS resources identified within the vicinity of the project.

Karen Wood, a qualified professional historian who meets the Secretary of the Interior Standards, conducted a site visit of the project area on August 13, 2019. She walked and drove the project area and the APE documenting above-ground resources. She investigated the APE for the existence of any historical properties, structures, objects, or districts listed in or eligible for listing in the NRHP. All resources that will be at least 50

years of age at the time of the project letting (estimated 2022) were surveyed and photographic documentation of “Contributing” resources and representative “Non-Contributing” resources was prepared. Aside from Bridge No. 026-38-03430 (NBI 7040), no resources in the APE were found to be listed in or eligible for the NRHP.

The following parties/agencies were invited to become consulting parties (CPs) to this project and were sent early coordination information dated June 28, 2019. The State Historic Preservation Officer (SHPO) is an automatic consulting party; that office and others that accepted consulting party status are shown in boldface type below. All consulting party correspondence is located in Appendix C.

**Indiana State Historic Preservation Officer
Indiana Landmarks Eastern Regional Office**

Jay County Historian

Jay County Historical Society

Mayor of Portland

Portland Street Department

Portland Historic Preservation Commission

Jay County Commissioners

Jay County Highway Department

Dr. James Cooper

Historic Spans Task Force

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

Delaware Tribe of Indians, Oklahoma

Forest County Potawatomi Community

In a letter dated July 23, 2019, SHPO staff stated “We are not aware of anyone who should be invited to become a consulting party for the purposes of the review of this project under Section 106, beyond those whom INDOT already has invited.”

In a letter dated July 24, 2019, the Miami Tribe of Oklahoma accepted consulting party status.

In an email dated February 13, 2020, Indiana Landmarks – Eastern Regional Office accepted consulting party status.

Per the terms of the “Programmatic Agreement Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridges PA), the FHWA-Indiana Division will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridge through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Because Bridge No. 026-38-03430 (NBI 7040) is a “Non-Select” bridge, the procedures outline in Stipulation III.B. of the Historic Bridges PA will be followed to fulfill FHWA’s Section 106 responsibilities for the project. (A copy of the Historic Bridges PA can be downloaded here: <http://www.in.gov/indot/2530.htm>).

Per Stipulation III.A.3 a Purpose and Need statement and Historic Bridge Alternatives Analysis (HBAA) was prepared and distributed to consulting parties on March 3, 2020. The summary of the HBAA is located in Appendix E. The most current plans are located in Appendix F.

In a letter dated March 30, 2020, the SHPO staff provided some comments in response to the HBAA:

SHPO comment: “Because it is apparent the bridge that carries SR 26 over the Salamonie River will be demolished, we request, pursuant to the Indiana Historic Bridges PA, that this bridge be photographically documented prior to commencement of the project by a qualified professional historian, architectural historian, or architect. Please provide overall views of the bridge and representative photographs of its deck, abutments, piers, along with any additional character defining features, including the arches. The documentation shall be produced in keeping with the applicable standards of the “Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards” (copy enclosed), except that photographic prints, a written description of the property, a statement of significance and drawings are not required in this case. We request that our office be provided with a draft of the digital photographs on a CD or DVD, including a photo key, for our review and comment. Following our comments, one CD or DVD of the documentation shall be provided to the Indiana SHPO for transmittal to the Indiana State Archives, and that a duplicate CD or DVD to be provided to at least one local public or not-for-profit organization in Jay County that agrees to retain the CD or DVD permanently and make it available to the public. Please advise us of the name and location of the public or not-for-profit organization that is willing to accept the CD or DVD.”

Response: Photographic documentation of the SR 26 over Salamonie River, Bridge 026-38-03430 A (NBI 007040) was completed by INDOT staff in December 2020 and submitted to the SHPO in February 2021. When transmitting the photographs to the SHPO, INDOT informed them that the Jay County Historical Society has agreed to retain the final images on disc permanently and make the disc available to the public.

SHPO comment: “Unless a responsible party steps forward to take ownership of the bridge and relocate it, we understand that it is likely that the bridge will be demolished. We see in Section VI. Minimization and Mitigation, subsection D. states that “INDOT will salvage elements that may be stored and used for future repair of similar historic bridges” - can this be done for portions of the bridge left in good condition or of elements unique to this bridge type regardless if a responsible party is identified during the marketing period? Is there value to store pieces or elements in the case a future party steps forward?”

Response: Per the Historic Bridges PA, salvage of elements that may be stored and used for future repair of similar historic bridges is only necessary if a party to accept those elements was identified during the bridge marketing. INDOT does not intend to salvage and store any elements of this bridge unless a responsible party comes forward. INDOT already has in storage, in their entirety, several truss bridges that were required to be stored per Memorandums of Agreement (MOAs) pre-dating the Historic Bridges PA. INDOT does not desire to store any additional bridges at this time.

On March 31, 2020, Rob Weaver with the Jay County Historical Society emailed Green 3, LLC (now SJCA Inc.) to make sure the Pioneer cemetery was being taken into account for this project since the markers are no longer present, but the graves are still in place. Green 3 responded, stating that the cemetery will be taken into account and efforts will be made to avoid it in regards to this project.

A Historic Properties Report (HPR) (Wood, November 2020) was completed for this project and recommended Bridge No. 026-38-03430A (NBI 7040) to remain eligible for listing in the NRHP. The HPR was sent to consulting parties on November 5, 2020. The summary of the HPR is found in Appendix D.

On November 23, 2020, SHPO staff responded to the HPR concurring with the conclusions of the HPR that the bridge carrying SR 26 over the Salamonie River (Bridge 026-38-03430A, NBI 007040) is the only above-ground historic property eligible for inclusion in the National Register of Historic Places (NRHP) within the APE.

With regards to archaeology, Christopher Jackson, M.S., RPA, conducted a Phase 1a archaeological reconnaissance investigation (Jackson, January 6, 2021). The investigation identified one site that was recommended not eligible for the listing on the NRHP and it was recommended no further archaeological work should be undertaken. The archaeology report was sent to consulting parties (SHPO and tribes only) on January 7, 2021.

In a letter dated February 8, 2021, SHPO staff concurred with the findings of the archaeology report stating “we concur with the opinion of the archaeologist, as expressed in the Phase 1a archaeological literature review and field reconnaissance survey report (Jackson, 01/06/2021), that Site 12-Ja-0700 (which was identified during these archaeologic investigations) does not appear eligible for inclusion in the NRHP.”

None of the other consulting parties provided any additional comments regarding the early coordination letter, HBAA, HPR, or archaeological investigation. Please see Appendix C for Consulting Party Correspondence.

Per the marketing requirements of the Historic Bridges PA, on March 9, 2020, two public notices were published in the *Indianapolis Star* and *The Commercial Review* offering the bridge to interested responsible parties for the rehabilitation and reuse, the storage and future reuse, or salvage elements of the bridge. The bridge was placed on the INDOT Historic Bridges Marketing Program website on February 12, 2020, and marketing signs were posted on the west and east sides of the bridge. No responses have been received to date. See appendix G for the marketing documentation for the bridge.

The procedures outlined in the Historic Bridge PA will be followed to fulfill FHWA’s Section 106 responsibilities for the bridge and any effects to the bridge. Therefore, the finding for this project only applies to other resources within the APE. Regarding these other resources, INDOT, acting on behalf of FHWA, has determined a “No Historic Properties Affected” finding is appropriate because no other resources in the APE are listed in or eligible for the NRHP.

A public notice of the “No Historic Properties Affected” finding will be published in *The Commercial Review* (Portland, IN) and the public will be afforded thirty (30) days to respond. This document will be revised, if necessary, after the expiration of the public comment period. Per Stipulation III(B)(5) of the Historic Bridges PA, INDOT will hold a public hearing for the project prior to completion of the National Environmental Policy Act (NEPA) studies. Consulting parties will be notified of the public hearing. Interested parties have until the end of the public hearing comment period to offer a reuse proposal for the bridge.

3. BASIS FOR FINDING

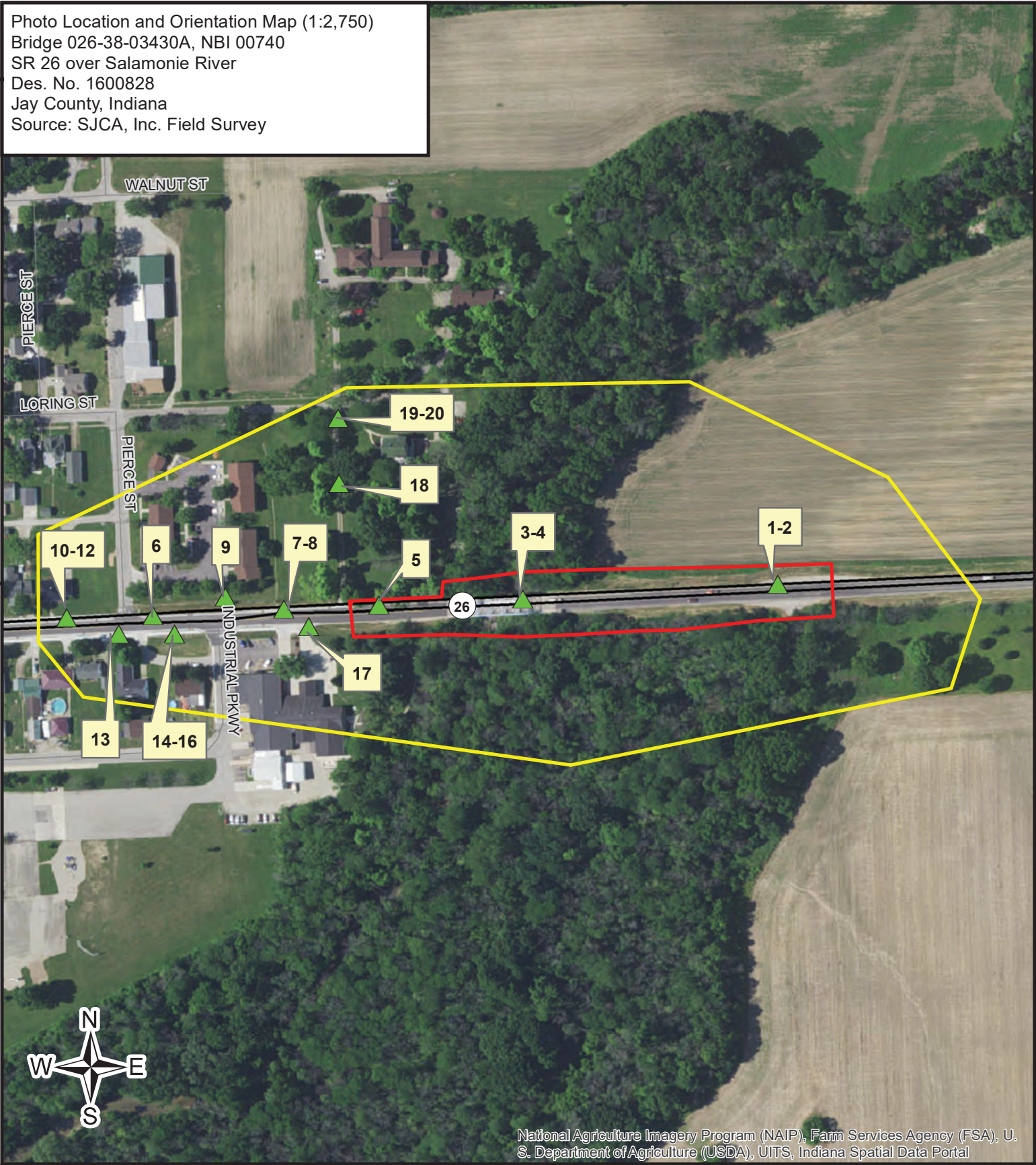
Bridge No. 026-38-03430A (NBI 7040) is the only resource in the APE that is listed in or eligible for listing in the NRHP. FHWA will satisfy its Section 106 responsibilities involving “Select” and “Non-Select” bridges through the Historic Bridge PA Project Development Process (PDP) (Stipulation III). Therefore, the finding for this project only applies to other resources within the APE and not the bridge. Because there are no other historic properties in the APE, INDOT, acting on behalf of FHWA, has determined a finding of “No Historic Properties Affected” is appropriate.

APPENDICES

- A – Maps
- B – Photographs
- C – Consulting Parties List and Correspondence
- D – Historic Property Report Summary
- E – Alternatives Analysis Summary
- F – Most Current Plans
- G – Marketing Measures

Note to Reader: The plan sheets and maps that were included in this effect finding document were removed to prevent duplication.

Photo Location and Orientation Map (1:2,750)
Bridge 026-38-03430A, NBI 00740
SR 26 over Salamonie River
Des. No. 1600828
Jay County, Indiana
Source: SJCA, Inc. Field Survey



0 160 320
Feet

- ▲ Photo Location
- ▭ Project Area
- ▭ APE





1. Facing west along SR 26 toward project area at eastern end of APE



2. Facing east along SR 26 toward eastern end of APE



3. Facing west along SR 26 at center of project area



4. Facing east along SR 26 at center of project area



5. Facing east along SR 26 toward bridge at western end of project area



6. Facing west along SR 26 toward western end of APE



7. Facing southwest along SR 26 toward JRDS at western end of project area



8. Facing northeast along SR 26 toward Morton Hawkins House



9. Facing south along SR 26 toward commercial and residential properties at western end of APE



10. Facing north along SR 26 toward 828 Water St at western end of APE